

PROJECT NARRATIVE

Date:
February 27, 2024 - REVISED



Reference:
Enclave Companies – 78th
Street Apartments
Attention:
Elizabeth O'Day



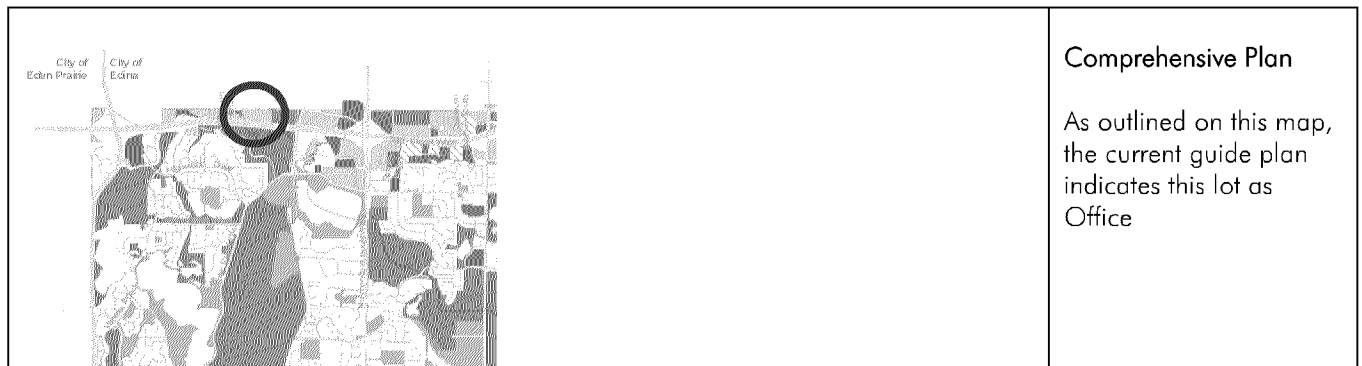
GENERAL DESCRIPTION

This proposal is for a 208 unit, market-rate apartment building located at 6701 78th Street W. As part of the Planned Development Application, this project will incorporate a separate one story non-residential commercial building of approximately 11,000 square feet. The site is currently vacant and was formerly the home of a Lifetime Fitness facility.

The proposed apartment building will be a 5 story wood framed Type IIIA construction type that will have one level of below grade parking in a non-combustible Type IA garage. The exterior materials of the building will be a blend of brick masonry, metal siding and cement fiber board. Please refer to sheet 6.0-6.5 to review the building exteriors. Surface parking will be provided onsite for residents and guests and the Commercial building will have a specific area designated for parking although there will be areas where shared surface parking is permitted. The below grade parking garage will be reserved for apartment residents only.

1. CITY COMPREHENSIVE PLAN

The current 2040 Comprehensive Plan indicates that this parcel is to be Office use. The legal description for the lot is "Lot 1, Block 1, Nine Mile West 2nd Addition."



Comprehensive Plan

As outlined on this map, the current guide plan indicates this lot as Office

2. ZONING

This site is currently zoned as C-4 which has High Density Residential listed as a Permitted Use

Legal Description

Lot 1, Block 1, Nine Mile West 2nd Addition
PID # 1771162121004

Density

Per table 21.301.01 a C-4 district requires a minimum FAR of .4 and a maximum of 2.0. The current proposed building has an FAR of .74

Building Area

The current total gross building area equals 299,383 SF. Each floor value is listed on sheet 1.0 – Project Data.

Building Height

Per Section 21.301.10 and also based off the city of Bloomington's Height Limit map, the current site falls within a "No Limit" zone and therefore does not have any height restrictions imposed on this development. In accordance with MNSBC Table 503 the maximum height allowed is 85'-0" (Type IIIA – sprinkler increase, R-2 occupancy).

Parking Requirements

Vehicular: Per Section 21.301.06 the minimum parking requirements for Multiple-Family Residence is 1.6 spaces per dwelling unit for 1BRs and Efficiency units and 2.0 spaces per dwelling unit for 2BR and larger units.

Per this requirement, 370 spaces are required. However, this project will be providing 9% affordable units and utilize the Opportunity Housing Ordinance incentives that allow for a 10% reduction in parking count as well as a 10% conversion allowance for covered parking to surface parking. After these incentives, the total required amount of parking for the apartment building is 333.

In addition to the apartment building, the proposed commercial building is being considered a daycare for the purposes of parking planning. The city code requires 1 stall per caregiver and 1.2 stalls per 10 program participants. Since a daycare provider has yet to be selected, our team is basing our estimate on historical data for daycares that are roughly 11,000 SF. As such, we anticipate 30 caregivers and 150 students for a total requirement of 48 stalls.

The total required parking on the site is 381 stalls and the plans are proposing 358 (210 enclosed and 148 surface) and will require a request for flexibility from the planning department. Being that the two programs demand parking at different times of the day, there is an opportunity for the apartment building and daycare parking demands to be served by the 148 surface stalls that are provided on the site.

3. PRELIMINARY AND FINAL PLAT

The project will require replatting as the daycare building will be designated as its own parcel.

4. PRELIMINARY DEVELOPMENT PLAN (PDP) & FINAL DEVELOPMENT PLAN

This Planned Development will require a Preliminary and Final Development Plan

5. CONDITIONAL USE PERMIT

The proposed commercial building use will be a daycare facility which will require a Conditional Use Permit as part of the amended zoning ordinance.

6. EXTERIOR MATERIALS

The exterior cladding materials shall comply with Section 19.63.08. The materials are currently identified on Sheets 6.0-6.5 included in this submission. Glass, Brick Masonry, Metal Siding (22 gauge) and Cement Fiber Board will make up the exterior of the building. As required by the zoning district, the street facing facades will be comprised of 85% primary materials and 15% secondary materials. Utilizing the Opportunity Housing Ordinance incentives, the non-street facing facades will be comprised of no less than 50% primary materials.

7. UNIT STORAGE

Resident storage will be provided on each level at the area indicated on the plans. Storage spaces will be no less than 96 cubic feet and pursuant to the Opportunity Housing Ordinance incentives the project will be required to provide 50% of the total number of units, or 104 storage spaces.

GENERAL PROJECT DESCRIPTION

1. EXISTING LAND USE

Currently, this parcel is a vacant lot.

2. PHASING AND CONSTRUCTION SCHEDULE

Pending the city review process, the current project schedule will begin construction Summer 2024

3. DEVELOPMENT METHOD

Project will be developed, owned and managed by Enclave Companies.

4. LEGAL INSTRUMENTS.

A survey had been provided in the entitlement drawings.

5. LIGHTING

A site lighting plan will be submitted to the city at a later date prior to the Planning Commission meeting in accordance with Section 21.307.07.

6. STORM WATER CALCS

See storm water report attached.

7. WATERSHED DISTRICT

Project will be required to submit for approval to the 9 Mile Creek Watershed District.

8. GEOTECHNICAL REPORT

See attached report #03392-010 from WSB

FLEXIBILITY REQUESTS

1. Minimum Non-Residential Floor Area Requirement in C-4 District

- a. Required non-residential floor area ratio is listed as .20 in the city code which would require 61,646 SF of non-residential space. The current proposal includes a 11,000 SF commercial building and 6,226 SF of assembly/common areas within the building. From a development

standpoint, providing 61,000+ SF of shell commercial space does not financially make sense as the demand for commercial/office use in the Minneapolis metro is amongst the lowest in the country. Rather than eliminating the commercial component from this proposed development all together, the development team has worked with their commercial broker to identify what type of commercial space has the greatest chance of being utilized and found that there is currently a need for more daycare facilities in this market.

2. Vehicle Parking Requirements

- a. As stated in section 2. Zoning above, the proposed development is providing 348 parking stalls, which is below the 381 required stalls. Being that the daycare and residential uses have different peak parking demand times, the developer is seeking flexibility that would allow for the two programs to share surface parking. Currently, there are 148 surface parking stalls available to serve both buildings.

ATTACHMENT A-
Geotechnical Report

ATTACHMENT B-
Stormwater Management Plan