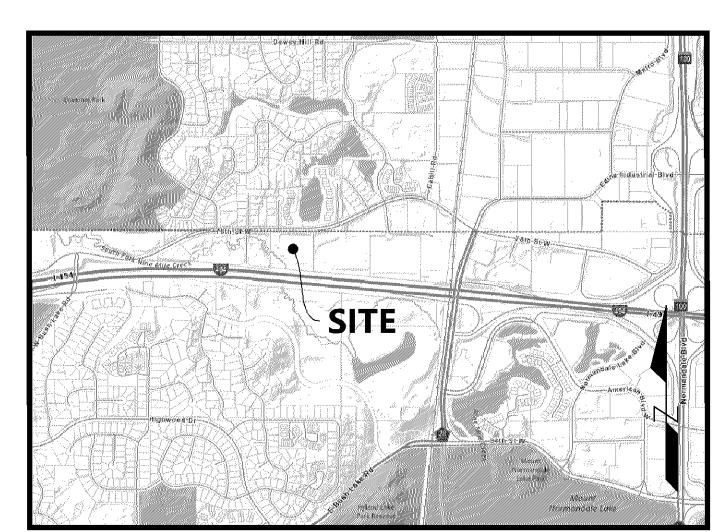
PL202300193 PL2023-193

PRELIMINARY DEVELOPMENT PLANS



Vicinity Map

FOR

ENCLAVE BLOOMINGTON 78TH STREET

BLOOMINGTON, MN

PREPARED FOR:

ENCLAVE DEVELOPMENT

300 23RD AVE E, SUITE 300 FARGO, ND 58078 CONTACT: BRIAN BOCHMAN

PHONE: 701-212-8110

EMAIL: BRIAN@ENCLAVECOMPANIES.COM

PREPARED BY:

Westwood

 Phone
 (952) 937-5150
 12701 Whitewater Drive, Suite #300

 Fax
 (952) 937-5822
 Minnetonka, MN 55343

 Toll Free
 (888) 937-5150
 westwoodps.com

 Westwood Professional Services, Inc.

PROJECT NUMBER: 0043990.00 CONTACT: SHARI LYNN S. AHRENS

SHEET INDEX

Sheet Number	Sheet Title
C001	COVER
C100	EXISTING CONDITIONS & REMOVALS PLAN
C101	TREE INVENTORY PLAN
C200	SITE PLAN
C201	PRELIMINARY PLAT
C300	GRADING PLAN
C301	WETLAND BUFFER PLAN
C400	EROSION CONTROL PLAN
C500	SANITARY SEWER & WATERMAIN
C501	STORM SEWER PLAN
C600	CITY DETAILS
C601	SITE DETAILS
L100	LANDSCAPE PLAN

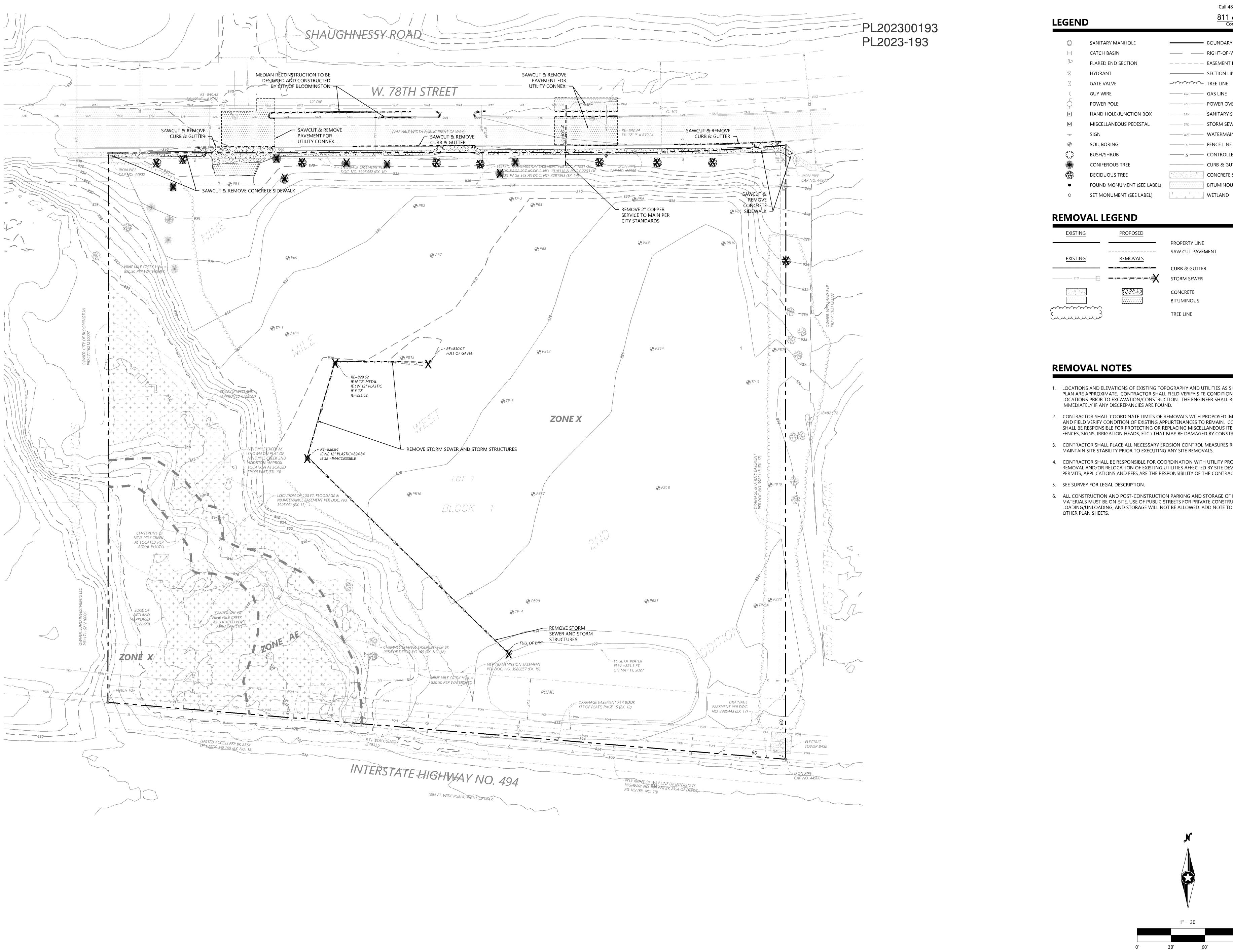
NO.	DATE	REVISION	SHEETS
	12/14/2023	CITY & WATERSHED COMMENTS	ALL
	12/29/2023	ISSUED FOR PERMIT	
	01/10/2024	CITY RESUBMITTAL	C200, C300, C601
	•		
	•		
	-	-	

PRELIMINARY
DEVELOPMENT PLANS

FOR

ENCLAVE BLOOMINGTON 78TH STREET BLOOMINGTON, MN

INITIAL SUBMITTAL DATE: 11/14/2023 SHEET: C001



Call 48 Hours before digging: 811 or call811.com
Common Ground Alliance

©	SANITARY MANHOLE		BOUNDARY LINE
	CATCH BASIN		RIGHT-OF-WAY LINE
	FLARED END SECTION		EASEMENT LINE
©	HYDRANT		SECTION LINE
\boxtimes	GATE VALVE	_~~~~	TREE LINE
(GUY WIRE	——————————————————————————————————————	GAS LINE
φ (POWER POLE	——— ÞoH ———	POWER OVERHEAD
HH	HAND HOLE/JUNCTION BOX		SANITARY SEWER
M	MISCELLANEOUS PEDESTAL		STORM SEWER
***************************************	SIGN		WATERMAIN
	SOIL BORING	χ	FENCE LINE
	BUSH/SHRUB	Δ	CONTROLLED ACCESS
*	CONIFEROUS TREE		CURB & GUTTER
	DECIDUOUS TREE		CONCRETE SURFACE
•	FOUND MONUMENT (SEE LABEL)		BITUMINOUS SURFACE

<u>EXISTING</u>	PROPOSED	
		PROPERTY LINE
		SAW CUT PAVEMENT
<u>EXISTING</u>	REMOVALS	
		CURB & GUTTER
STO —	— \— \— \### \— \— \# #	STORM SEWER
		CONCRETE
		BITUMINOUS
Emm		TREE LINE

- 1. LOCATIONS AND ELEVATIONS OF EXISTING TOPOGRAPHY AND UTILITIES AS SHOWN ON THIS PLAN ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY SITE CONDITIONS AND UTILITY LOCATIONS PRIOR TO EXCAVATION/CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED
- 2. CONTRACTOR SHALL COORDINATE LIMITS OF REMOVALS WITH PROPOSED IMPROVEMENTS AND FIELD VERIFY CONDITION OF EXISTING APPURTENANCES TO REMAIN. CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING OR REPLACING MISCELLANEOUS ITEMS (SUCH AS FENCES, SIGNS, IRRIGATION HEADS, ETC.) THAT MAY BE DAMAGED BY CONSTRUCTION.
- 3. CONTRACTOR SHALL PLACE ALL NECESSARY EROSION CONTROL MEASURES REQUIRED TO MAINTAIN SITE STABILITY PRIOR TO EXECUTING ANY SITE REMOVALS.
- 4. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH UTILITY PROVIDERS FOR REMOVAL AND/OR RELOCATION OF EXISTING UTILITIES AFFECTED BY SITE DEVELOPMENT. ALL PERMITS, APPLICATIONS AND FEES ARE THE RESPONSIBILITY OF THE CONTRACTOR.
- 6. ALL CONSTRUCTION AND POST-CONSTRUCTION PARKING AND STORAGE OF EQUIPMENT AND MATERIALS MUST BE ON-SITE. USE OF PUBLIC STREETS FOR PRIVATE CONSTRUCTION PARKING, LOADING/UNLOADING, AND STORAGE WILL NOT BE ALLOWED. ADD NOTE TO SITE PLAN AND

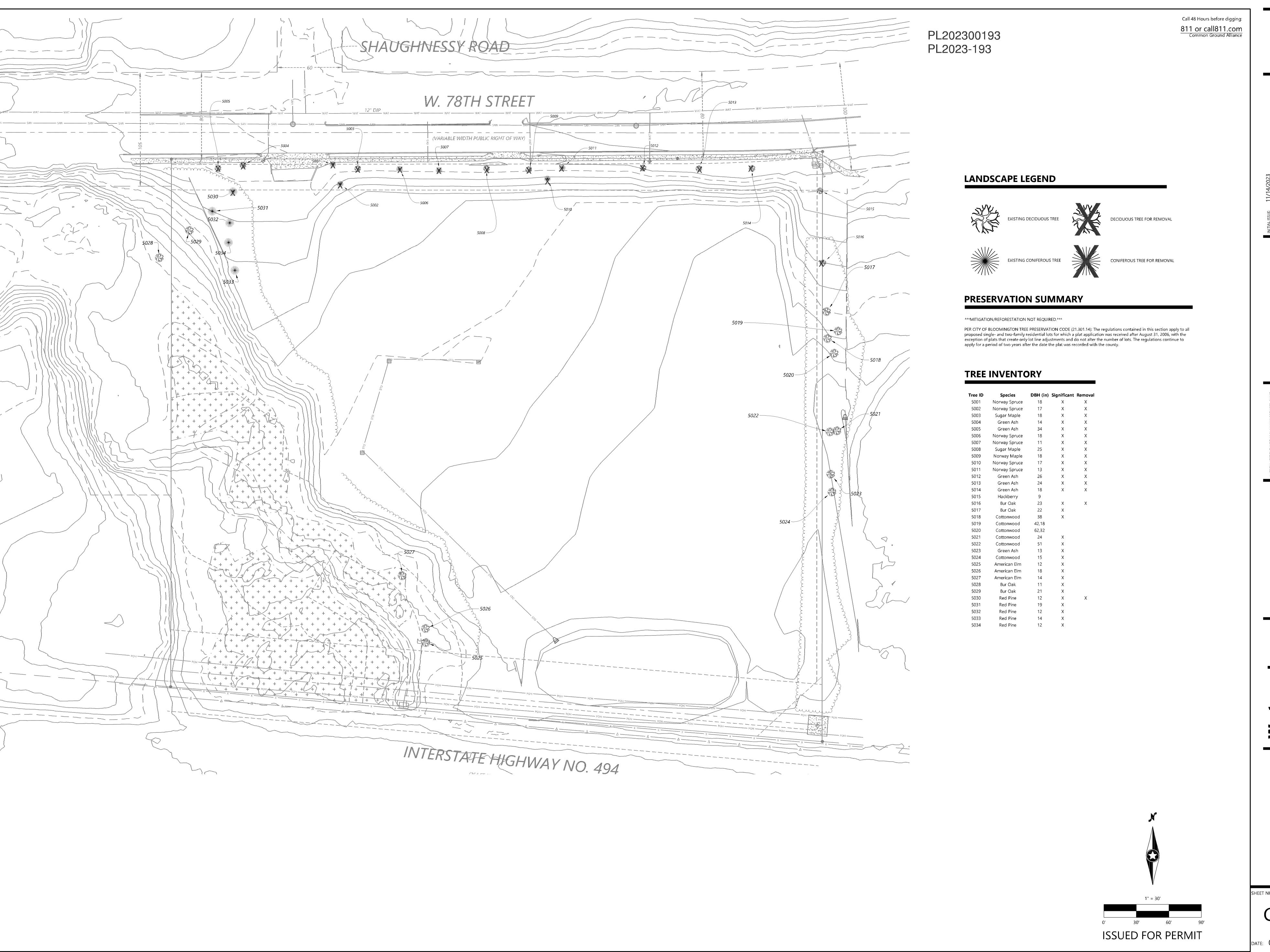
ISSUED FOR PERMIT

Phone Fax Toll Free

SHEET NUMBER: C100

PROJECT NUMBER: 0043990.00

DATE: 01/10/2024



DESIGNED: SSA
CHECKED: SSA
DRAWN: AKW
HORIZONTAL SCALE: 30'
VERTICAL SCALE: 6' or 3'

SUE: 11/14/2023
IS: 714/2023 CITY & WATERSHED COMMENTS 729/2023 ISSUED FOR PERMIT 710/2024 CITY RESUBMITTAL

ENCLAVE DEVELOPMENT 300 23RD AVE E, SUITE 300 FARGO, ND 58078

DATE: 01/10/2024 LICENSE NO. 44018

STREET
BLOOMINGTON, MN

 Phone
 (952) 937-5150
 12701 Whitewater Drive, Suite Fax
 (952) 937-5150
 westwoodps.com

 Toll Free
 (888) 937-5150
 westwoodps.com

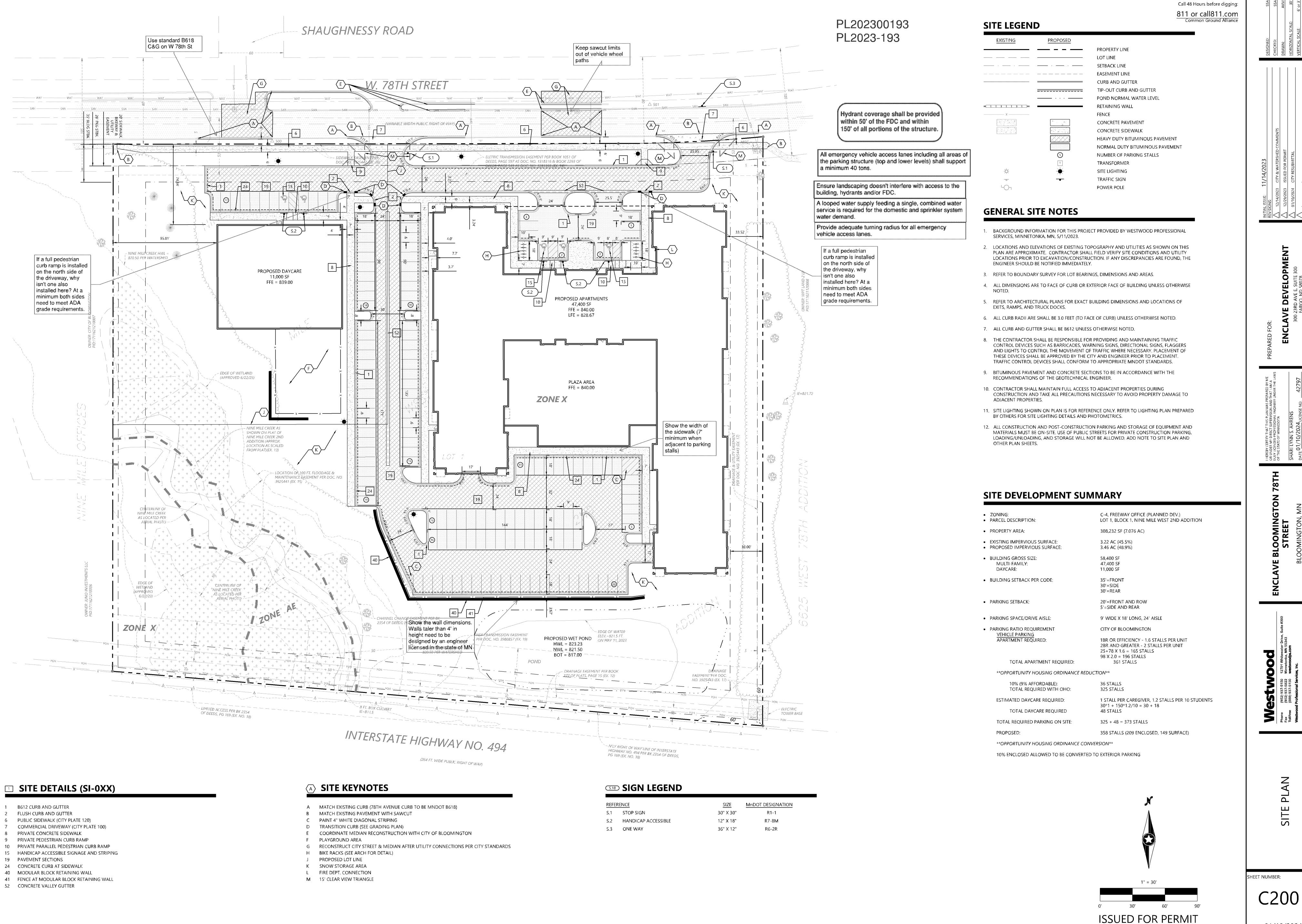
 Westwood Professional Services, Inc.

EE INVENTORY PLAN

HEET NUMBER:

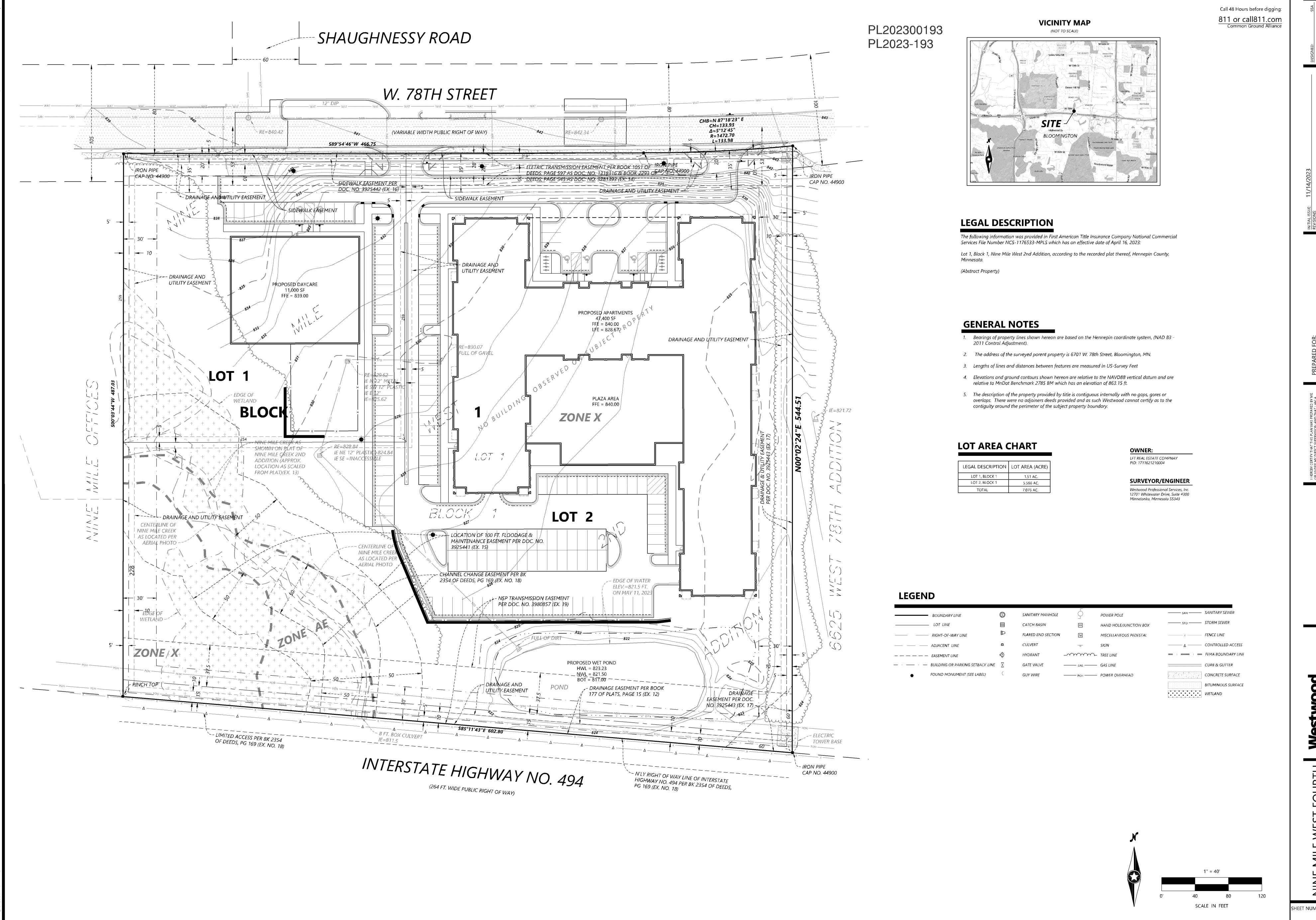
C101

DATE: 01/10/2024



C200

DATE: 01/10/2024



 DESIGNED:
 SSA

 CHECKED:
 SSA

 DRAWN:
 AKW

 HORIZONTAL SCALE:
 30'

 VERTICAL SCALE:
 6' or 3'

ENCLAVE DEVELOPMEN'
300 23RD AVE E, SUITE 300
FARGO, ND 58078

OK UNDER MY DIRECT SUPERVISION AND THAIT AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

SHARI LYNN S. AHRENS

DATE: 01/10/2024 LICENSE NO. 42797

STREET
BLOOMINGTON, MN

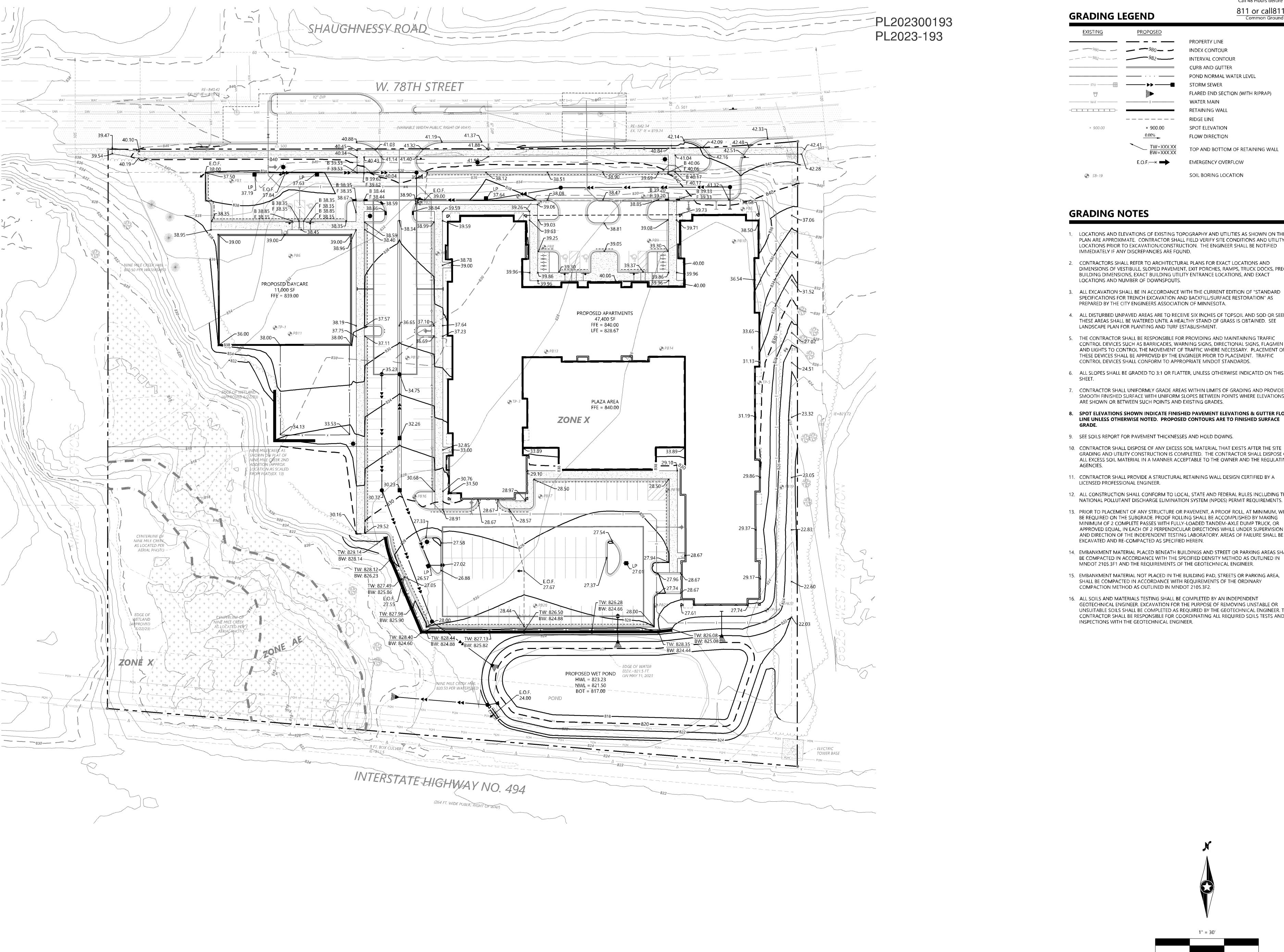
e (952) 937-5150 12701 Whitewater Drive, Suite (952) 937-5822 Minnetonka, MN 55343 ree (888) 937-5150 westwoodps.com wood Professional Services, Inc.

INE MILE WEST FOURTH ADDITION PRELIMINARY PLAT

SHEET NUMBER:

C201

DATE: 01/10/2024



Call 48 Hours before digging: 811 or call811.com
Common Ground Alliance

EXISTING	PROPOSED	
		PROPERTY LINE
manage 980 more manage	<u> </u>	INDEX CONTOUR
	982-	INTERVAL CONTOUR
		CURB AND GUTTER
	· · · ·	POND NORMAL WATER LEVEL
STO —		STORM SEWER
		FLARED END SECTION (WITH RIPRAP)
		WATER MAIN
		RETAINING WALL
		RIDGE LINE
× 900.00	× 900.00	SPOT ELEVATION
	0.00%	FLOW DIRECTION
	$\frac{\text{TW=XXX.XX}}{\text{BW=XXX.XX}}$	TOP AND BOTTOM OF RETAINING WALL
	E.O.F.— →	EMERGENCY OVERFLOW
		COUL POPUNG LOCATION

- 1. LOCATIONS AND ELEVATIONS OF EXISTING TOPOGRAPHY AND UTILITIES AS SHOWN ON THIS LOCATIONS PRIOR TO EXCAVATION/CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED
- 2. CONTRACTORS SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULE, SLOPED PAVEMENT, EXIT PORCHES, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS, EXACT BUILDING UTILITY ENTRANCE LOCATIONS, AND EXACT
- ALL EXCAVATION SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR TRENCH EXCAVATION AND BACKFILL/SURFACE RESTORATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA.
- 4. ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE SIX INCHES OF TOPSOIL AND SOD OR SEED. THESE AREAS SHALL BE WATERED UNTIL A HEALTHY STAND OF GRASS IS OBTAINED. SEE LANDSCAPE PLAN FOR PLANTING AND TURF ESTABLISHMENT.
- 5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. TRAFFIC CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MNDOT STANDARDS.
- 6. ALL SLOPES SHALL BE GRADED TO 3:1 OR FLATTER, UNLESS OTHERWISE INDICATED ON THIS
- 7. CONTRACTOR SHALL UNIFORMLY GRADE AREAS WITHIN LIMITS OF GRADING AND PROVIDE A SMOOTH FINISHED SURFACE WITH UNIFORM SLOPES BETWEEN POINTS WHERE ELEVATIONS
- 8. SPOT ELEVATIONS SHOWN INDICATE FINISHED PAVEMENT ELEVATIONS & GUTTER FLOW LINE UNLESS OTHERWISE NOTED. PROPOSED CONTOURS ARE TO FINISHED SURFACE
- 9. SEE SOILS REPORT FOR PAVEMENT THICKNESSES AND HOLD DOWNS.
- GRADING AND UTILITY CONSTRUCTION IS COMPLETED. THE CONTRACTOR SHALL DISPOSE OF ALL EXCESS SOIL MATERIAL IN A MANNER ACCEPTABLE TO THE OWNER AND THE REGULATING
- 11. CONTRACTOR SHALL PROVIDE A STRUCTURAL RETAINING WALL DESIGN CERTIFIED BY A
- 12. ALL CONSTRUCTION SHALL CONFORM TO LOCAL, STATE AND FEDERAL RULES INCLUDING THE
- 13. PRIOR TO PLACEMENT OF ANY STRUCTURE OR PAVEMENT, A PROOF ROLL, AT MINIMUM, WILL BE REQUIRED ON THE SUBGRADE. PROOF ROLLING SHALL BE ACCOMPLISHED BY MAKING MINIMUM OF 2 COMPLETE PASSES WITH FULLY-LOADED TANDEM-AXLE DUMP TRUCK, OR APPROVED EQUAL, IN EACH OF 2 PERPENDICULAR DIRECTIONS WHILE UNDER SUPERVISION AND DIRECTION OF THE INDEPENDENT TESTING LABORATORY. AREAS OF FAILURE SHALL BE
- 14. EMBANKMENT MATERIAL PLACED BENEATH BUILDINGS AND STREET OR PARKING AREAS SHALL BE COMPACTED IN ACCORDANCE WITH THE SPECIFIED DENSITY METHOD AS OUTLINED IN MNDOT 2105.3F1 AND THE REQUIREMENTS OF THE GEOTECHNICAL ENGINEER.
- 15. EMBANKMENT MATERIAL NOT PLACED IN THE BUILDING PAD, STREETS OR PARKING AREA, SHALL BE COMPACTED IN ACCORDANCE WITH REQUIREMENTS OF THE ORDINARY COMPACTION METHOD AS OUTLINED IN MNDOT 2105.3F2.
- GEOTECHNICAL ENGINEER. EXCAVATION FOR THE PURPOSE OF REMOVING UNSTABLE OR UNSUITABLE SOILS SHALL BE COMPLETED AS REQUIRED BY THE GEOTECHNICAL ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOILS TESTS AND

ISSUED FOR PERMIT

Phone Fax Toll Free

C300

date: 01/10/2024

ALL NON-FUNCTIONAL BMPS - OBSERVED CONDITION; SEDIMENT OVERTOPPING, UNDER WATER, SCOURED ENDS, UNDERMINED, DESTROYED, NON-FUNCTION AS DESIGNED, ETC. - SHALL BE MAINTAINED OR REPLACED BY

PERIMETER SEDIMENT CONTROL (SILT FENCE, FIBER LOGS, BERMS, ETC.) - OBSERVED CONDITION TO BE 1/2 FULL OF SEDIMENT, FLATTENED TO 1/2 HEIGHT, DRIVEN OVER, UNDERMINED, SCOURED, MOVED FOR ACCESS, ETC. -

INLET PROTECTION BMPS, CONVEYANCES, SURFACE WATERS - OBSERVED CONDITION; SEDIMENT DEPOSITION, SEDIMENT DELTAS AN ACCUMULATION OF SEDIMENT MATERIAL, DEVICES APPEAR PLUGGED WITH SEDIMENT -

TEMPORARY SEDIMENT BASINS AND TRAPS/PERMANENT SEDIMENT BASINS - OBSERVED TO HAVE SEDIMENT DEPOSITION AND ACCUMULATION TO 1/2 OF THE STORAGE VOLUME - CLEAN OUT, REMOVE ACCUMULATED

5. SITE EXIT LOCATIONS, ROCK EXIT PADS, OTHER ANTI-TRACKING PRACTICES - OBSERVED TO HAVE ACCUMULATED SEDIMENT IN ROCK OR OTHER ANTI-TRACKING BMP, TRACKING OF SEDIMENT FROM THE SITE ONTO PAVED

6. PAVED SURFACES AND ADJACENT STREETS - OBSERVED TO BE TRACKED WITH SEDIMENT AND SOIL MATERIAL FRO THE SITE HAULING OR ACCESS - SWEEP WITHIN 1 CALENDAR DAY OF DISCOVERY, ADDITIONAL AND/OR MORE

SHALL BE MAINTAINED, REPAIRED OR SUPPLEMENTATION OF PERIMETER SEDIMENT CONTROL SHOULD BE DONE BY THE END OF NEXT BUSINESS DAY OR AD FIELD CONDITIONS ALLOW.

SURFACES - TOP DRESS ROCK, MAINTAIN ROCK EXIT OR OTHER ANTI-TRACKING CONTROLS, SCRAP PAVED SURFACES, SWEEP PAVED SURFACES WITHIN 1 CALENDAR DAY OF DISCOVERY.

REMOVAL/CLEAN OUT OF ACCUMULATED SEDIMENT AND DELTAS TO BE REMOVED WITHIN 7 DAYS, STABILIZE AS NEEDED IF SOILS ARE EXPOSED DURING REMOVAL/CLEAN OUT.

THE END OF THE NEXT BUSINESS DAY AFTER DISCOVERY OR NOTIFICATION, OR AS SOON AS FIELD CONDITIONS ALLOW.

FREQUENT SWEEPING MAY BE NEEDED TO MAINTAIN PUBLIC SAFETY OR PREVENT WASHING FROM FORECASTED RAINS.

SEDIMENT MATERIAL WITHIN 7 DAYS OF OBSERVATION, OR AS FIELD CONDITIONS ALLOW ACCESS.

INLET PROTECTION

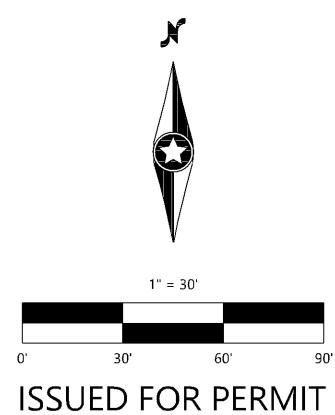
TREE PROTECTION FENCE

GENERAL EROSION CONTROL NOTES

- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF SHALL NOT BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL VERIFY DISCREPANCIES.
- 2. ALL SILT FENCE AND OTHER EROSION CONTROL FEATURES SHALL BE IN-PLACE PRIOR TO ANY EXTREME IMPORTANCE TO BE AWARE OF CURRENT FIELD CONDITIONS WITH RESPECT TO EROSION CONTROL. TEMPORARY PONDING, DIKES, HAYBALES, ETC., REQUIRED BY THE CITY
- 3. EROSION AND SILTATION CONTROL (ESC): THE CONTRACTOR SHALL ASSUME COMPLETE RESPONSIBILITY FOR CONTROLLING ALL SILTATION AND EROSION OF THE PROJECT AREA. THE CONTRACTOR SHALL USE WHATEVER MEANS NECESSARY TO CONTROL THE EROSION AND SILTATION INCLUDING BUT NOT LIMITED TO: CATCH BASIN INSERTS, CONSTRUCTION ENTRANCES, EROSION CONTROL BLANKET, AND SILT FENCE. ESC SHALL COMMENCE WITH GRADING AND CONTINUE THROUGHOUT THE PROJECT UNTIL ACCEPTANCE OF THE WORK BY THE OWNER. THE CONTRACTOR'S RESPONSIBILITY INCLUDES ALL IMPLEMENTATION AS REQUIRED TO PREVENT EROSION AND THE DEPOSITING OF SILT. THE OWNER MAY DIRECT THE CONTRACTOR'S METHODS AS DEEMED FIT TO PROTECT PROPERTY AND IMPROVEMENTS, ANY DEPOSITION OF SILT OR MUD ON NEW OR EXISTING PAVEMENT OR IN EXISTING STORM SEWERS OR SWALES SHALL BE REMOVED AFTER EACH RAIN EVENT. AFFECTED AREAS SHALL BE CLEANED TO THE SATISFACTION OF THE OWNER, ALL AT THE EXPENSE OF THE CONTRACTOR. ALL TEMPORARY EROSION CONTROL SHALL BE REMOVED BY THE CONTRACTOR AFTER THE TURF IS ESTABLISHED.
- 4. ALL STREETS DISTURBED DURING WORKING HOURS MUST BE CLEANED AT THE END OF EACH WORKING DAY. A CONSTRUCTION ENTRANCE TO THE SITE MUST BE PROVIDED ACCORDING TO DETAILS TO REDUCE TRACKING OF DIRT ONTO PUBLIC STREETS.
- 5. PROPOSED PONDS SHALL BE EXCAVATED FIRST AND USED AS TEMPORARY PONDING DURING CONSTRUCTION.
- 6. WHEN INSTALLING END-OF-LINE FLARED END SECTIONS, BRING THE SILT FENCE UP & OVER THE FLARED END SECTIONS & COVER DISTURBED AREAS WITH RIP RAP. THE UPSTREAM FLARED END
- SECTIONS SHALL HAVE WOOD FIBER BLANKET INSTALLED ON THE DISTURBED SOILS. 7. ALL UNPAVED AREAS ALTERED DUE TO CONSTRUCTION ACTIVITIES MUST BE RESTORED WITH SEED AND MULCH, SOD, EROSION CONTROL BLANKET OR BE HARD SURFACE WITHIN 2 WEEKS
- 8. THE SITE MUST BE STABILIZED PER THE REQUIREMENTS OF THE MPCA, NPDES, MNDOT, AND
- A. TEMPORARY (GREATER THAN 1-YEAR) SEED SHALL BE MNDOT SEED MIX 22-111 AT 30.5-POUNDS PER ACRE.
- TEMPORARY (LESS THAN 1-YEAR) SEED SHALL BE MNDOT SEED MIX 21-112 (FALL) OR 21-111 (SPRING/SUMMER) AT 100-POUNDS PER ACRE
- C. INFILTRATION/FILTRATION BASIN SHALL BE MNDOT SEED MIX 34-262 AT 14.5-POUNDS PER
- D. POND SLOPES SHALL BE MNDOT SEED MIX 33-261 AT 35-POUNDS PER ACRE.
- E. GENERAL SEEDING SHALL BE MNDOT SEED MIX 25-151 AT 70-POUNDS PER ACRE.
- F. MULCH SHALL BE MNDOT TYPE 1 APPLIED AT 2-TONS PER ACRE.

OF COMPLETION OF CONSTRUCTION.

- 9. FOR AREAS WITH SLOPE OF 3:1 OR GREATER, RESTORATION WITH SOD OR EROSION CONTROL BLANKET IS REQUIRED.
- 10. ALL TEMPORARY STOCKPILES MUST HAVE SILT FENCE INSTALLED AROUND THEM TO TRAP SEDIMENT.
- 11. ALL PERMANENT PONDS USED AS TEMPORARY SEDIMENT BASINS DURING CONSTRUCTION SHALL BE DREDGED AFTER THE SITE HAS BEEN STABILIZED TO RESTORE THE POND TO THE PROPOSED BOTTOM ELEVATION.
- 12. ALL CONSTRUCTION SHALL CONFORM TO LOCAL AND STATE RULES INCLUDING THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS.
- 13. THE SITE MUST BE KEPT IN A WELL-DRAINED CONDITION AT ALL TIMES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY DITCHES, PIPING OR OTHER MEANS REQUIRED TO INSURE PROPER DRAINAGE DURING CONSTRUCTION. LOW POINTS IN ROADWAYS OR BUILDING PADS MUST BE PROVIDED WITH A POSITIVE OUTFLOW.



SHEET NUMBER:

DATE: 01/10/2024

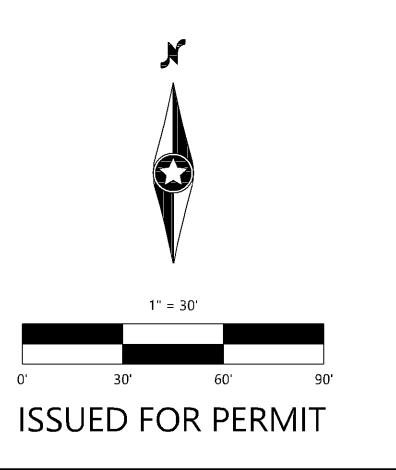
Call 48 Hours before digging: 811 or call811.com
Common Ground Alliance

SANITARY SEWER AND WATERMAIN LEGEND

EXISTING	PROPOSED	
		PROPERTY LINE
 		EASEMENT LINE
		CURB AND GUTTER
 SAN	3	SANITARY SEWER
 STO		STORM SEWER
 WAT		WATER MAIN
 WAT		HYDRANT
 GAS	GAS	GAS
 PUE		UNDERGROUND ELECTRIC
 		OVERHEAD ELECTRIC
 TUG	— ТОН —	UNDERGROUND TELEPHONE
 TOH	НОТ	OVERHEAD TELEPHONE
 FO		TELEPHONE FIBER OPTIC
 CTV		CABLE TELEVISION
	H	GATE VALVE
型		FLARED END SECTION (WITH RIPRAP)
+		LIGHT POLE

GENERAL WATERMAIN & SANITARY SEWER NOTES

- 1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS ARE BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND LIMITED MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION SHALL NOT BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND NOTIFY THE OWNER OR ENGINEER OF
- ALL SANITARY SEWER AND WATER MAIN MATERIAL AND INSTALLATIONS SHALL BE PER CITY REQUIREMENTS, MINNESOTA PLUMBING CODE, AND IN ACCORDANCE WITH THE CURRENT AND SANITARY SEWER AND STORM SEWER INSTALLATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN THE NECESSARY FEDERAL, STATE AND LOCAL PERMITS FOR THE PROPOSED WORK OR VERIFY WITH THE OWNER OR ENGINEER THAT PERMITS HAVE BEEN OBTAINED. PERMIT FEES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR UNLESS OTHERWISE ARRANGED WITH THE OWNER.
- 4. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATION AND DIMENSIONS OF DOORWAYS, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY CONNECTION LOCATIONS.
- 5. ALL PRIVATE UTILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE SPECIFICATIONS OF THE APPROPRIATE UTILITY COMPANY. THE CONTRACTOR SHALL COORDINATE THE SERVICE LINE CONSTRUCTION WITH THE UTILITY COMPANIES.
- 6. CONTRACTOR SHALL OBTAIN ALL NECESSARY CITY PERMITS FOR UTILITY CONNECTIONS, AND UTILITIES SHALL BE INSPECTED AND APPROVED BY THE CITY. THE CITY SHALL BE NOTIFIED 48-HOURS PRIOR TO COMMENCING WITH THE UTILITY CONSTRUCTION OR ANY REQUIRED TESTING. CONTRACTOR SHALL NOT OPERATE, INTERFERE WITH, CONNECT ANY PIPE OR HOSE TO, OR TAP ANY WATER MAIN BELONGING TO THE CITY UNLESS DULY AUTHORIZED TO DO SO BY THE CITY. ANY ADVERSE CONSEQUENCES OF SCHEDULED OR UNSCHEDULED DISRUPTIONS OF SERVICE TO THE PUBLIC ARE TO BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 7. WATER MAIN LENGTHS AS SHOWN ARE APPROXIMATE HORIZONTAL LENGTHS. ALLOW FOR ADDITIONAL PIPE WHEN INSTALLING ON SLOPES OR WHEN DEFLECTIONS ARE REQUIRED. THE JOINT DEFLECTIONS SHALL NOT EXCEED THE MAXIMUM RECOMMENDED BY THE PIPE MANUFACTURER OR BY LOCAL GOVERNING SPECIFICATIONS. FITTINGS REQUIRED TO CONSTRUCT WATER MAIN SHALL BE INCLUDED IN WATER MAIN CONSTRUCTION.
- 8. PROVIDE WATER MAIN THRUST RESTRAINTS PER CITY STANDARD REQUIREMENTS.
- 9. A MINIMUM VERTICAL SEPARATION OF 18 INCHES IS REQUIRED AT ALL WATER LINE CROSSINGS WITH SANITARY SEWER OR STORM SEWER. THE WATER LINE SHALL NOT HAVE JOINTS OR CONNECTION WITHIN 10-FEET OF THE CROSSING. INSULATE CROSSINGS WITH STORM SEWER.
- 10. UTILITY SERVICES TYPICALLY TERMINATE 5' OUTSIDE BUILDING WALL UNLESS OTHERWISE
- 11. DUCTILE IRON WATER LINES SHALL BE CLASS 52, PER AWWA C115 OR C151. COPPER WATER LINES SHALL BE TYPE K PER ASTM B88. PVC WATER LINES SHALL BE PER AWWA C900 AND INSTALLED PER AWWA C605 IF ALLOWED BY CITY.
- 12. ALL WATER LINES SHALL HAVE 8' MINIMUM COVER, INSULATE WATER MAIN IF LESS THAN 8' OF COVER. INSULATION SHALL BE DOW STYROFOAM HI BRAND 35 OR EQUIVALENT, WITH 4
- 13. SANITARY SEWER PIPE OUTSIDE THE BUILDING ENVELOPE SHALL BE POLYVINYL CHLORIDE (PVC) SDR 35 OR 26. SDR 26 IS REQUIRED FOR DEPTHS GREATER THAN 15 FEET. SANITARY SEWER PIPE WITHIN 5 FEET OF THE BUILDING AND UNDER FOOTINGS SHALL BE SCHEDULE 40 PER ASTM D2665. ALL PLASTIC SANITARY SEWER SHALL BE INSTALLED PER D2321. SOLVENT WELD JOINTS MUST INCLUDE USE OF A PRIMER WHICH IS OF A CONTRASTING COLOR TO THE PIPE AND CEMENT. ALL SANITARY SEWER SHALL BE TESTED ACCORDING TO MINNESOTA PLUMBING
- 14. ALL NONCONDUCTIVE PIPE SHALL BE INSTALLED WITH A LOCATE (TRACER) WIRE PER MINNESOTA PLUMBING CODE, PART 604.9.
- 15. AFTER CONSTRUCTION IS COMPLETED, THE CONTRACTOR SHALL PROVIDE THE OWNER WITH AN AS-BUILT RECORD OF UTILITY CONSTRUCTION. THE AS-BUILT SHALL INCLUDE LOCATION AND LENGTH DEVIATIONS OR CHANGES TO THE PLAN. CONTRACTOR TO VERIFY WITH OWNER OR ENGINEER WHETHER A PLAN WITH POST-CONSTRUCTION ELEVATIONS IS REQUIRED.
- 16. ALL MANHOLE CASTINGS IN PAVED AREAS SHALL BE SUMPED 0.05 FEET. RIM ELEVATIONS ON



HEET NUMBER: C500

date: 01/10/2024

STMH-512

= IE=835.83 (S)

IE=835.83 (W)

19 LF-12" STM

SWR @ 0.90%

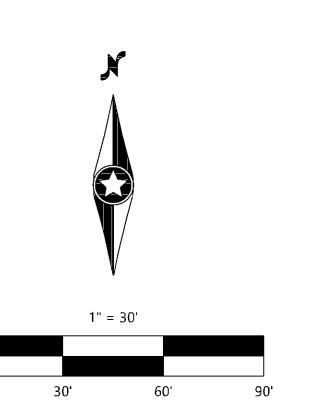
RE=841.40

GENERAL STORM SEWER NOTES

- 1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS ARE BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND LIMITED MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION SHALL NOT BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND NOTIFY THE OWNER OR ENGINEER OF DISCREPANCIES.
- 2. ALL STORM SEWER MATERIAL AND INSTALLATIONS SHALL BE PER CITY REQUIREMENTS, MINNESOTA PLUMBING CODE, AND IN ACCORDANCE WITH THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION AND SANITARY SEWER AND STORM SEWER INSTALLATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN THE NECESSARY FEDERAL, STATE AND LOCAL PERMITS FOR THE PROPOSED WORK OR VERIFY WITH THE OWNER OR ENGINEER THAT PERMITS HAVE BEEN OBTAINED. PERMIT FEES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR UNLESS OTHERWISE ARRANGED WITH THE OWNER.
- 4. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATION AND DIMENSIONS OF DOORWAYS, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY CONNECTION LOCATIONS.
- 5. ALL PRIVATE UTILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE SPECIFICATIONS OF THE APPROPRIATE UTILITY COMPANY. THE CONTRACTOR SHALL COORDINATE THE SERVICE LINE CONSTRUCTION WITH THE UTILITY COMPANIES.
- 6. CONTRACTOR SHALL OBTAIN ALL NECESSARY CITY PERMITS FOR UTILITY CONNECTIONS, AND UTILITIES SHALL BE INSPECTED AND APPROVED BY THE CITY. THE CITY SHALL BE NOTIFIED 48-HOURS PRIOR TO COMMENCING WITH THE UTILITY CONSTRUCTION OR ANY REQUIRED TESTING. CONTRACTOR SHALL NOT OPERATE, INTERFERE WITH, CONNECT ANY PIPE OR HOSE TO, OR TAP ANY WATER MAIN BELONGING TO THE CITY UNLESS DULY AUTHORIZED TO DO SO BY THE CITY. ANY ADVERSE CONSEQUENCES OF SCHEDULED OR UNSCHEDULED DISRUPTIONS OF SERVICE TO THE PUBLIC ARE TO BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 7. A MINIMUM VERTICAL SEPARATION OF 18 INCHES IS REQUIRED AT ALL WATER LINE CROSSINGS WITH STORM SEWER. THE WATER LINE SHALL NOT HAVE JOINTS OR CONNECTION WITHIN 10-FEET OF THE CROSSING. INSULATE CROSSINGS WITH STORM SEWER.
- 8. UTILITY SERVICES TYPICALLY TERMINATE 5' OUTSIDE BUILDING WALL UNLESS OTHERWISE SHOWN OR NOTED.

9. STORM SEWER PIPE:

- A. RCP AND HDPE PIPE MAY BE INSTALLED WITH APPROVAL OF LOCAL GOVERNING AGENCY. B. REINFORCED CONCRETE PIPE SHALL BE CLASS 5 FOR PIPE DIAMETERS 18" AND SMALLER, CLASS 3 FOR PIPE DIAMETERS 21" AND LARGER UNLESS OTHERWISE NOTED, PER ASTM C76, WITH GASKETS PER ASTM C443.
- C. HDPE STORM PIPE 4- TO 10-INCHES IN DIAMETER SHALL MEET REQUIREMENTS OF AASHTO M252. HDPE STORM PIPE 12- TO 60-INCHES IN DIAMETER SHALL MEET REQUIREMENTS OF ASTM F2306. FITTINGS SHALL BE PER ASTM D3212 AND INSTALLED PER ASTM D2321.
- D. PVC STORM SEWER PIPE AND FITTINGS SHALL BE SCHEDULE 40 PIPE PER ASTM D2665 AND
- INSTALLED PER ASTM D2321. E. CORRUGATED METAL PIPE (CMP) FOR SIZES 18- TO 120-INCH AND MUST MEET ASTM A760 OR ASTM A796 AND BE INSTALLED PER ASTM A798. CMP MAY NOT BE INSTALLED WITHIN 10-FEET OF A WATERMAIN, WATER SERVICE, OR A BUILDING.
- F. ALL STORM SEWER JOINTS AND STRUCTURE CONNECTIONS SHALL BE GASTIGHT OR WATERTIGHT AS REQUIRED BY MINNESOTA PLUMBING CODE, PART 707.3. STORM SEWER LOCATED WITHIN 10-FEET OF A BUILDING AND/OR WATER LINE SHALL BE TESTED PER MINNESOTA PLUMBING CODE, PART 712.
- G. HDPE PIPE CONNECTIONS INTO ALL CONCRETE STRUCTURES MUST BE MADE WITH WATER TIGHT MATERIALS UTILIZING AN A-LOK OR WATERSTOP GASKET OR BOOT, CAST-IN-PLACE RUBBER BOOT, OR APPROVED EQUAL. WHERE THE ALIGNMENT PRECLUDES THE USE OF THE ABOVE APPROVED WATERTIGHT METHODS, CONSEAL 231 WATERSTOP SEALANT, OR APPROVED EQUAL WILL ONLY BE ALLOWED AS APPROVED BY THE ENGINEER.
- 10. ALL NONCONDUCTIVE PIPE SHALL BE INSTALLED WITH A LOCATE (TRACER) WIRE PER MINNESOTA PLUMBING CODE, PART 604.9.
- 11. AFTER CONSTRUCTION IS COMPLETED, THE CONTRACTOR SHALL PROVIDE THE OWNER WITH AN AS-BUILT RECORD OF UTILITY CONSTRUCTION. THE AS-BUILT SHALL INCLUDE LOCATION AND LENGTH DEVIATIONS OR CHANGES TO THE PLAN. CONTRACTOR TO VERIFY WITH OWNER OR ENGINEER WHETHER A PLAN WITH POST-CONSTRUCTION ELEVATIONS IS REQUIRED.
- 12. ALL CATCH BASIN CASTINGS IN CURB SHALL BE SUMPED 0.15 FEET AND MANHOLE CASTINGS IN PAVED AREAS SHALL BE SUMPED 0.05 FEET. RIM ELEVATIONS ON PLAN REFLECT THE SUMPED ELEVATIONS.



Phone Fax Toll Free

HEET NUMBER:

C501

date: 01/10/2024

ISSUED FOR PERMIT

CB 2' x 3'-510 STORM SEWER STUB-513 RE=838.36 RE=837.49 TE=834.35 (E) IE=836.00 (N) 니 **STMH-500** IE=833.69 (E) VERIFY WITH MEP PB10 IE=834.35 (W) RE=839.11 IE=833.69 (W) √ IE=832.35 (N) IE=833.09 (E) IE=828.00 (S) NINE MILE CREEK HWL = S SUMP=825.00 820.50 PER WATERSHED PROPOSED DAYCARE 11,000 SF FFE = 839.00STORM SEWER STUB-401 STORM SEWER STUB-402 PROPOSED APARTMENTS IE=830.34 (E)-└-IE=830.68 (W) 47,400 SF VERIFY WITH MEP VERIFY WITH MEP FFE = 840.0014 LF-12" STM 23 LF-12" STM SWR @ 2.37% LFE = 828.67SWR @ 2.95% STMH-410 RE=834.71 -----IE=828.00 (S) IE=828.00 (N) STMH-113 SUMP=825.00 RE=835.28 + + + + + + + + - EDGE OF WETLAND IE=829.50 (S) (APPROVED 6/22/23) IE=829.50 (N) PLAZA AREA FFE = 840.0056 LF-18" STM __SWR @ 0.50% ZONEX ____ x _____ x ____ CBMH-411 -RE=832.09 - NINE MILE CREEK AS IE=828.28 (N) SHOWN ON PLAT OF NINE MILE CREEK 2ND ADDITION (APPROX. LOCATION AS SCALED STMH-112 CBMH-302 i FROM PLAT)(EX. 13) RE=830.72 -RE=830.08 IE=826.22 (N) IE=825.24 (S) IE=826.22 (S) >- LOCATION OF 100 FT. FLOODAGE & MAINTENANCE EASEMENT PER DOC. NO. 3925441 (EX. 15) CENTERLINE OF 11 LF-12" STM NINE MILE CREEK ✓ SWR @ 0.50% AS LOCATED PER AERIAL PHOTO -**CBMH-200** STMH-300 RE=826.86 RE=826.89 (528) SC-310 CHAMBERS IE=823.17 (W) −IE=823.17 (W) 13,062 SF FOOTPRINT ,605 CF STORAGE BELOW OUTLET IE=823.17 (E) IE=823.17 (E) CB 2' x 3'-301 | 100-YR HWL = 824.42 | SUMP=820.17 SUMP=823.17 RE+826.42 BOT = 823.17 NE=823.23 (N) RE=827.90 IE=823.23 (E) Pre-treatment required prior to IE=821.58 (S) STORM SEWER STUB-151 discharge to stormwater BMP IE=823.17 (N) -IE=824.00 (S) VERIFY WITH MEP - + +\+ + + ,4 + + + + .+ + + \ + + * CENTERLINE OF RE≒827.88 IE=823.00 (N)— 23 LF-12" STM IE=821.63\(SE) >+ + + + + + + + AERIAL PHOTO -SUMP=818.63 __SWR @ 11.09% 66 LF-30" STM 🔷 12" FES-150 . . . ____ ____ . SWR @_0.3.6% . -----4.4 CU YD CLASS III RIPRAP SWR @ 0.20% 12" FES-100 30" FES-110 IE=821.50 - CHANNEL CHANGE EASEMENT PER BK -4.4 CU YD CLASS III RIPRAP —12.3 CU YD €LASS III RIPRAP 2354 OF DEÈDS; PG 169 (EX. NO. 18) IE=821.50 IE=821.50 ~ EDGE OF WATER ELEV.=821.5 FT. 18" FES-702 - PER DOC. NO. 3980857 (EX. 19) ON MAY 11, 2023 + + + + + 6.4 GU YD CLASS III RIPRAP-` ~ NINE MILE CREEK HWL ≠ + + IE=820.50 820.50 PER WATERSHED RE=824.00 81 LF-18" STM SWR @ 0.20% / IE=821.50 (E) IE=820.66 (W) – DRAINAGE EASEMENT PER BOOK EASEMENT PER DOC. 177 OF PLATS, PAGE 15 (EX. 12) NO. 3925443 (EX. 17) 🚽

8 F1. BOX CULVERT IE=877.5

10 LF-18" STM SWR @ 0.00%

N'LY RIGHT OF WAY LINE OF INTERSTATE HIGHWAY NO. 494 PER BK 2354 OF DEEDS,

RE=840.39

TIE=832.59 (W)

IE=832.59 (S)

24 LF-15" STM

__ SWR @ 1.00%_

EX, 12" IE = 819.24

147 LF-12" STM SWR @ 1.00%

🛰 ELETRIC TRANSMISSION EASEMENT PER BOOK 🕏 DEEDS, PAGE 597 AS DOC. NO. 1318316 & BOOK 2

/ SWR @ 1.00% 66 LF-12" STM SWR @ 1.00%

RE=837.48

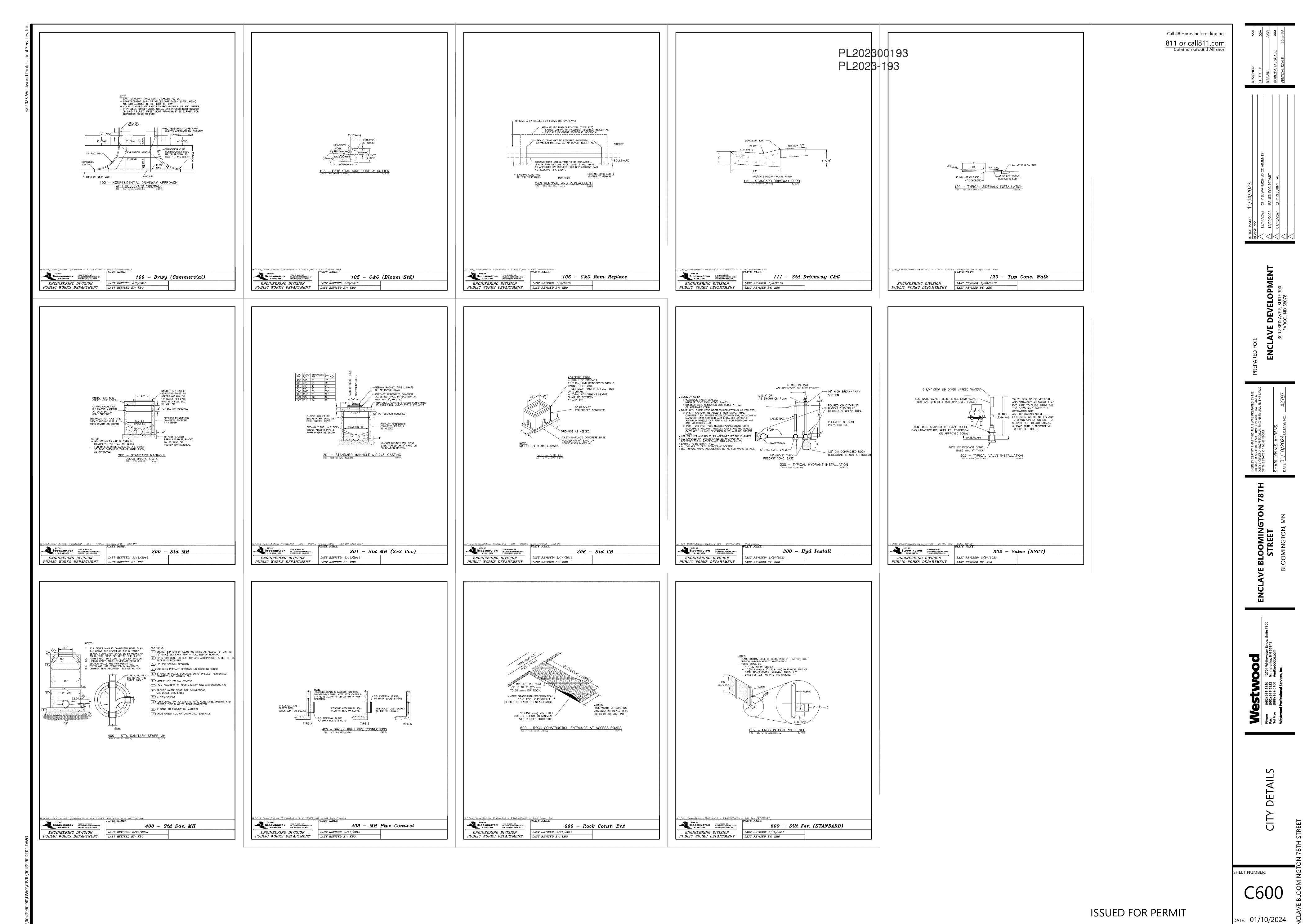
IE=833.46 (W)

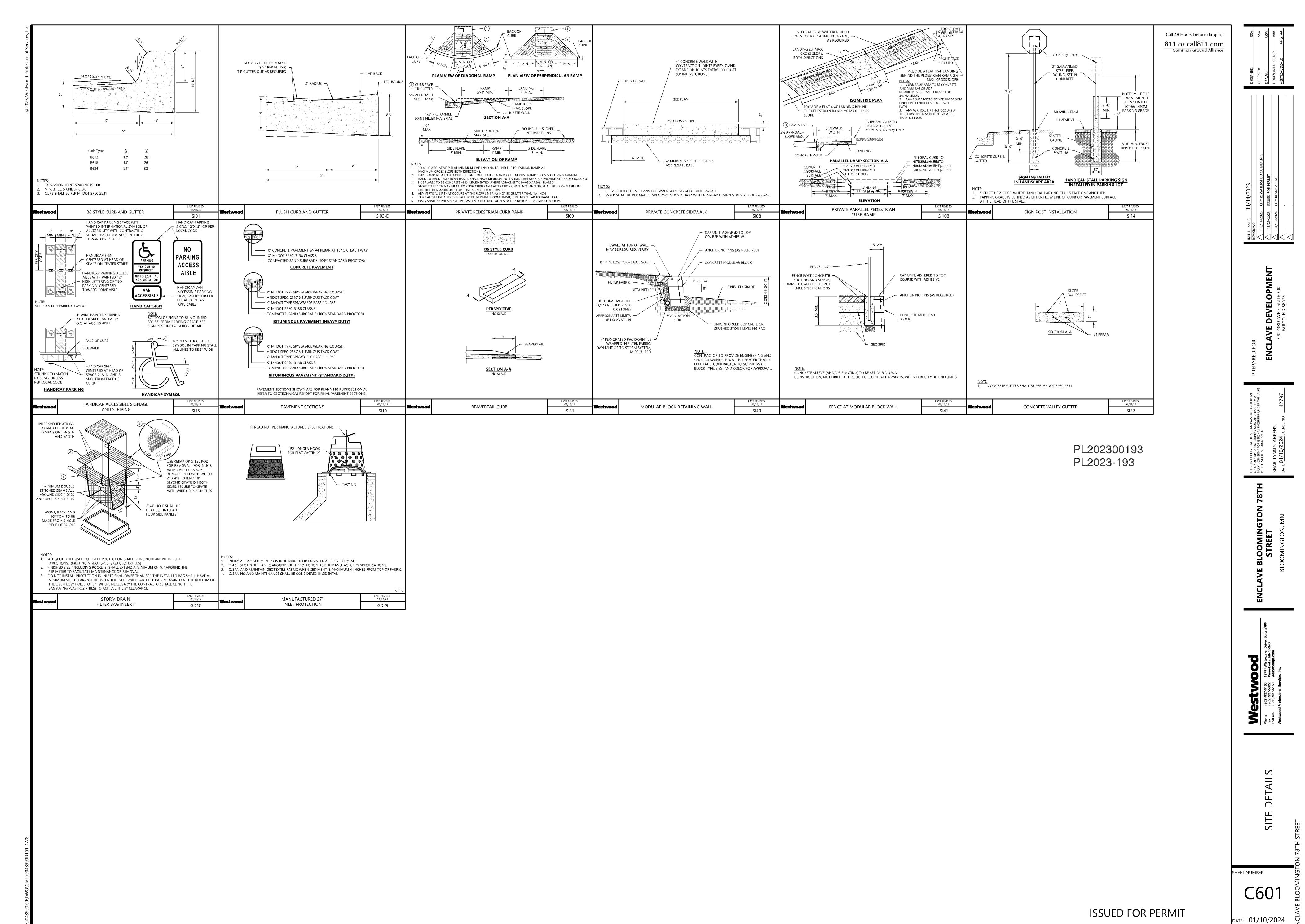
CB 2' x 3'-503

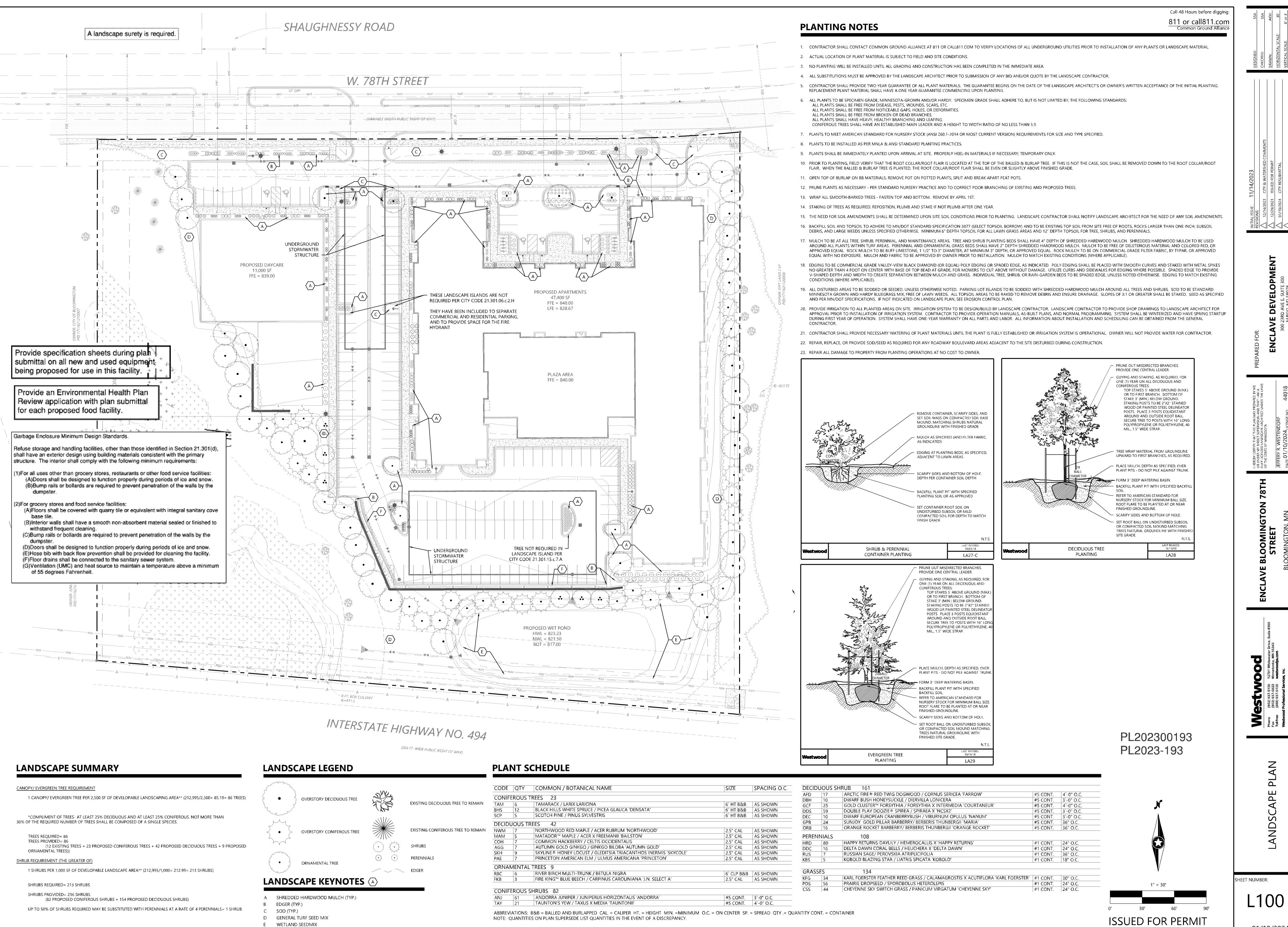
IE=834 00 (E)

∟RE=837.04

OF DEEDS, PG 169 (EX. NO. 18)







F ROCK MULCH

PROJECT NUMBER: 0043990.00

DATE: 01/10/2024