

CASE FILE #PL201900040

THE DISTRICT

Bloomington, MN

STUARTCO
YOUR HOME. OUR COMMITMENT.

UNITED PROPERTIES
CREATING DEEP ROOTS

Kimley»Horn

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767 EUSTIS STREET, SUITE 100,
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CITY OF
BLOOMINGTON
MINNESOTA

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I hereby certify that this document was
prepared by me or under my direct supervision
and that I am a duly licensed Engineer
under the laws of the State of Minnesota

Signature
Daniel L. Elenbaas, P.E.
Typed or Printed Name
44614 05-28-2018
License # Date

BUILDING PERMIT
SET
8/01/2019

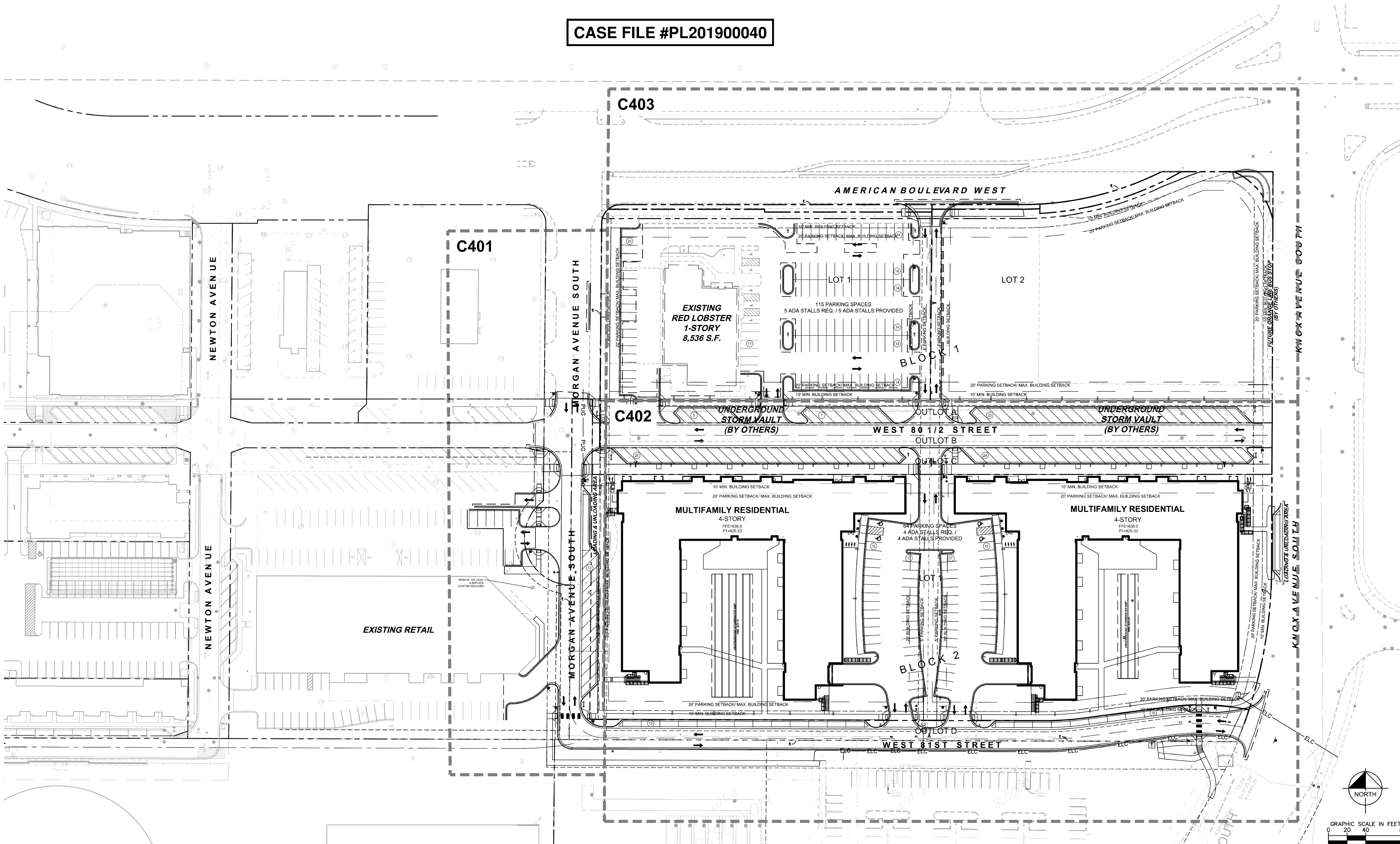
ORIGINAL ISSUE: 08/01/2019
REVISIONS
No. Description Date
1 CITY REVISIONS 08/21/2019
2 FINAL CITY REVISIONS 08/29/2019

160682003
PROJECT NUMBER

RAH
DRAWN BY: DLE
CHECKED BY:
THE DISTRICT

OVERALL SITE PLAN

C400



ZONING SUMMARY	
EXISTING ZONING	C-5 FREEWAY MIXED USE
PARKING SETBACKS	SIDE/REAR = 5' R.O.W. = 20'
BUILDING SETBACKS	FRONT = 10' MIN / 20' MAX R.O.W. = 20' / 30' BUILDINGS OVER FOUR STORIES REAR = 20'

BUILDING DATA SUMMARY		
	BUILDING GROSS SIZE	FLOOR AREA RATIO (FAR)
LOT 1, BLOCK 1 - RED LOBSTER	8,536 S.F.	0.12 FAR =(8,536/72,675)
LOT 1, BLOCK 2 - MULTI-FAMILY RESIDENTIAL	264,054 S.F.	1.39 FAR=(264,054/189,539)
TOTAL	272,590 S.F.	1.04 FAR =(272,590/262,214)

IMPERVIOUS / PERVIOUS SUMMARY		
	IMPERVIOUS AREA	PERVIOUS AREA
LOT 1, BLOCK 1 - RED LOBSTER REQUIRED	95% MAXIMUM	5% MINIMUM
LOT 1, BLOCK 1 - RED LOBSTER PROVIDED	61,993 S.F. (85.30%)	10,682 S.F. (14.70%)
LOT 2, BLOCK 1 REQUIRED	95% MAXIMUM	5% MINIMUM
LOT 2, BLOCK 1 PROVIDED	N/A	N/A
LOT 1, BLOCK 2 - MULTI-FAMILY RESIDENTIAL REQUIRED	85% MAXIMUM	15% MINIMUM
LOT 1, BLOCK 2 - MULTI-FAMILY RESIDENTIAL PROVIDED	127,002 S.F. (67.00%)	62,542 S.F. (33.00%)

PARKING SUMMARY			
	PROPOSED	REQUIRED / EXISTING	ADA REQUIRED / PROPOSED
LOT 1, BLOCK 1 - RED LOBSTER	115 SPACES	120 SPACES (EXISTING)	5 SPACES / 5 SPACES
LOT 2, BLOCK 1	N/A	N/A	N/A
WEST 80 1/2 STREET	83 SPACES	N/A	N/A
LOT 1, BLOCK 2 - MULTI-FAMILY RESIDENTIAL	258 SPACES (64 SPACES + 194 GARAGE SPACES)	435 SPACES TOTAL (248 GARAGE SPACES)+(156 ONE BEDROOM UNITS*1.8) + (92 TWO BEDROOM UNITS*1.8) + (0 THREE BEDROOM UNITS*1.8) + (0 FOUR BEDROOM UNITS*1.8) + (6,000 S.F. PARTY ROOM*100)	4 SPACES / 4 SPACES (EXTERIOR) 8 SPACES / 8 SPACES (GARAGE)
MORGAN CIRCLE	12 SPACES	N/A	N/A
WEST 81ST STREET	23 SPACES	N/A	N/A
TOTAL SPACES	491 SPACES	555 SPACES	
RED LOBSTER	-5 SPACES, NET LOSS		
KLOOT PROPERTY	+2 SPACES, NET GAIN		
PARKING SPACE/PRIVATE AISLE	9' WIDE X 18' LONG, 24' AISLE		

APPROVED
ZONING REVIEW ONLY
NMJ 11/14/2019

ENGINEERING DIVISION
Approved By: Julie Long, PE
09/04/2019 7:02:50 AM

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Bloomington, MN



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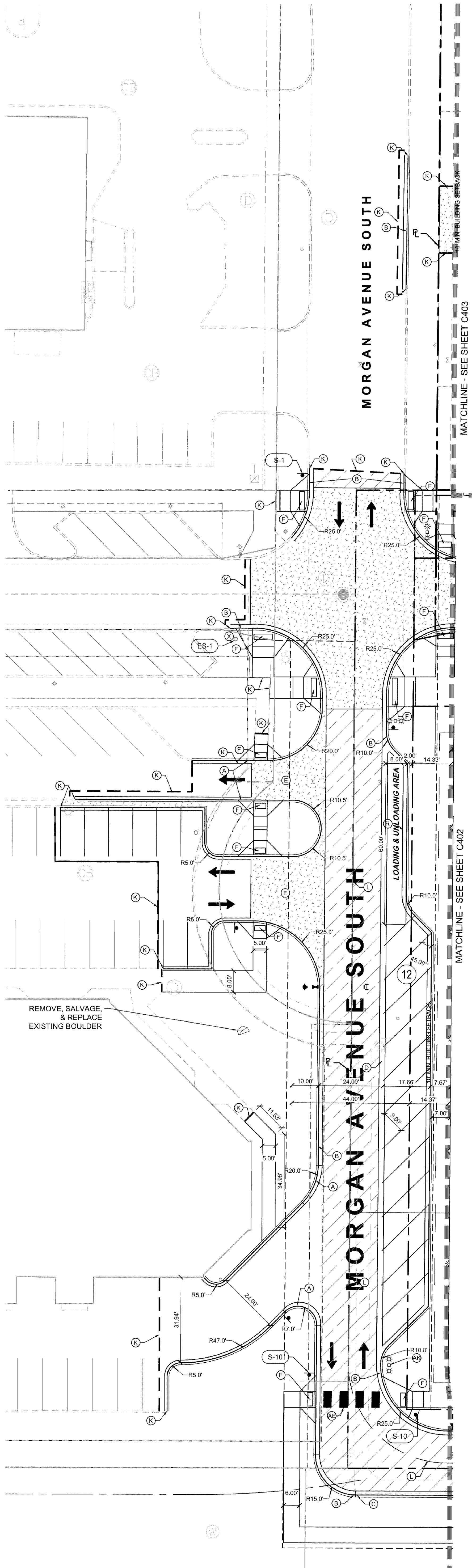
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THE DISTRICT

SITE PLAN - MORGAN AVENUE

C401



LEGEND

	PROPERTY LINE
	SETBACK LINE
	PROPOSED FENCE (TYP.)
	EASEMENT LINE
	SAWCUT LINE
	RETAINING WALL (TYP.)
	PROPOSED CURB AND GUTTER (TYP.)
	PROPOSED LIGHTPOLE (TYP.)
	PROPOSED SIGN (TYP.)
	PROPOSED TRANSFORMER (TYP.)
	PROPOSED JERSEY BARRIER (TYP.)
	PROPOSED HEAVY DUTY ASPHALT (TYP.)
	PROPOSED STANDARD DUTY ASPHALT (TYP.)
	PROPOSED CONCRETE PAVEMENT (TYP.)
	PROPOSED CONCRETE SIDEWALK (TYP.)
	PROPOSED COLORED CONCRETE SIDEWALK (TYP.)
	PROPOSED UNDERGROUND STORM VAULT (TYP.)
	PROPOSED PAVERS

SIGN LEGEND

KEYNOTE	NAME	MNDOT SIGN NUMBER	SIGN SIZE
S-1	STOP SIGN	R1-1	30" X 30"
S-2	HANDICAP ACCESSIBLE	R7-8M	12" X 18"
S-3	NO PARKING	R8-38	12" X 12"
S-4	DO NOT ENTER	R5-1	30" X 30"
S-5	BEGIN ONE WAY	R6-X1	24" X 30"
S-6	KEEP RIGHT	R4-7	24" X 30"
S-7	NO LEFT TURN SYMBOL	R3-2	24" X 24"
S-8	ONE WAY (IN ARROW)	R6-1 (R)	36" X 12"
S-9	RIGHT LANE MUST TURN RIGHT	R3-6 (R/L)	30" X 30"
S-10	PEDESTRIAN CROSSING	W11-2	30" X 30"
S-11	YIELD	R1-2	24" X 24"
S-12	ONLINE ORDER SIGN (FROM TENANT)	-	-
S-13	MENU BOARD	-	-
S-14	PRE-SALES BOARD	-	-
ES-1	EXISTING STOP SIGN TO REMAIN	-	-

KEYNOTE LEGEND

A	B612 CURB & GUTTER (SEE DETAIL) (TYP.)
B	B618 CURB & GUTTER (SEE DETAIL) (TYP.)
C	B1018 CURB & GUTTER (SEE DETAIL) (TYP.)
D	26" RIBBON CURB (SEE DETAIL) (TYP.)
E	CONCRETE DRIVEWAY APRON (SEE DETAIL) (TYP.)
F	PUBLIC PEDESTRIAN CURB RAMP (WITH TRUNCATED DOMES) (SEE DETAIL) (TYP.)
G	PUBLIC ACCESSIBLE DROPPED PEDESTRIAN RAMP (WITH TRUNCATED DOMES) (SEE DETAIL) (TYP.)
H	PRIVATE PEDESTRIAN CURB RAMP (WITHOUT TRUNCATED DOMES) (SEE DETAIL) (TYP.)
I	PRIVATE ACCESSIBLE DROPPED PEDESTRIAN RAMP (WITHOUT TRUNCATED DOMES) (SEE DETAIL) (TYP.)
J	ACCESSIBLE PARKING LOGO, STRIPING, & SIGNAGE (SEE DETAIL) (TYP.)
K	SAWCUT & MATCH EXISTING EDGE OF PAVEMENT/ CURB & GUTTER
L	PAVEMENT MARKING- 4" DASHED (10' LONG, 20' GAP), COLOR YELLOW
M	B624 CURB & GUTTER (SEE DETAIL) (TYP.)
N	BIKE RACK (SEE DETAIL) (TYP.)

SITE PLAN NOTES

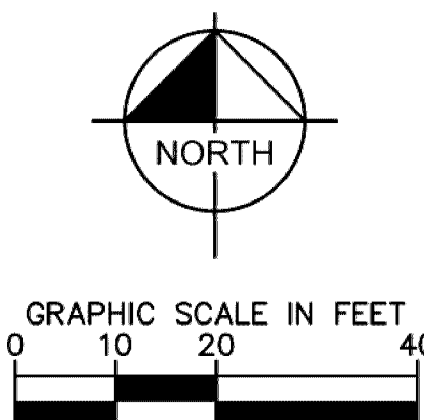
- ALL WORK AND MATERIALS SHALL COMPLY WITH ALL CITY/COUNTY REGULATIONS AND CODES AND O.S.H.A. STANDARDS.
- CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULES, SLOPE PAVING, SIDEWALKS, EXIT PORCHES, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
- ALL INNER CURBED RADII ARE TO BE 5' AND OUTER CURBED RADII ARE TO BE 10' UNLESS OTHERWISE NOTED. STRIPED RADII ARE TO BE 5'.
- ALL DIMENSIONS AND RADII ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
- EXISTING STRUCTURES WITHIN CONSTRUCTION LIMITS ARE TO BE ABANDONED, REMOVED OR RELOCATED AS NECESSARY. ALL COST SHALL BE INCLUDED IN BASE BID.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL RELOCATIONS, (UNLESS OTHERWISE NOTED ON PLANS) INCLUDING BUT NOT LIMITED TO, ALL UTILITIES, STORM DRAINAGE, SIGNS, TRAFFIC SIGNALS & POLES, ETC. AS REQUIRED. ALL WORK SHALL BE IN ACCORDANCE WITH GOVERNING AUTHORITIES REQUIREMENTS AND PROJECT SITE WORK SPECIFICATIONS AND SHALL BE APPROVED BY SUCH. ALL COST SHALL BE INCLUDED IN BASE BID.
- SITE BOUNDARY, TOPOGRAPHY, UTILITY AND ROAD INFORMATION TAKEN FROM A SURVEY BY SUNDE LAND SURVEYING, DATED 3/18/2016.
KIMLEY-HORN ASSUMES NO LIABILITY FOR ANY ERRORS, INACCURACIES, OR OMISSIONS CONTAINED THEREIN.
- PYLON / MONUMENT SIGNS SHALL BE CONSTRUCTED BY OTHERS. SIGNS ARE SHOWN FOR GRAPHICAL & INFORMATIONAL PURPOSES ONLY. CONTRACTOR TO VERIFY SIZE, LOCATION AND ANY REQUIRED PERMITS NECESSARY FOR THE CONSTRUCTION OF THE PYLON / MONUMENT SIGN.
- CONTRACTOR SHALL REFERENCE ARCH / MEP PLANS FOR SITE LIGHTING AND ELECTRICAL PLAN.
- NO PROPOSED LANDSCAPING SUCH AS TREES OR SHRUBS, ABOVE AND UNDERGROUND STRUCTURES, OR OTHER OBSTRUCTIONS SHALL BE LOCATED WITHIN EXISTING OR PROPOSED UTILITY EASEMENTS AND RIGHTS OF WAY UNLESS SPECIFICALLY NOTED ON PLANS OTHERWISE.
- REFERENCE ARCHITECTURAL PLANS FOR DUMPSTER ENCLOSURE DETAILS.
- REFER TO FINAL PLAT OR ALTA SURVEY FOR EXACT LOT AND PROPERTY BOUNDARY DIMENSIONS.
- ALL AREAS ARE ROUNDED TO THE NEAREST SQUARE FOOT.
- ALL DIMENSIONS ARE ROUNDED TO THE NEAREST TENTH FOOT.
- ALL PARKING STALLS TO BE 9' IN WIDTH AND 18' IN LENGTH WITH 24' AISLE UNLESS OTHERWISE INDICATED.
- FOR OFFSITE IMPROVEMENTS, SEE THE <OFFSITE PLANS> IMPROVEMENTS PLANS.
- CONTRACTOR SHALL OBTAIN A PUBLIC WORKS PERMIT FOR OBSTRUCTIONS AND CONCRETE WORK WITHIN THE RIGHT-OF-WAY. PERMIT IS REQUIRED PRIOR TO REMOVALS OR INSTALLATION. CONTACT LACEY KAARE (952-563-4555, LKAARE@BLOOMINGTONMN.GOV) FOR PERMIT INFORMATION.
- ALL CONSTRUCTION AND POST-CONSTRUCTION PARKING AND STORAGE OF EQUIPMENT AND MATERIALS MUST BE ON-SITE. USE OF PUBLIC STREETS FOR PRIVATE CONSTRUCTION PARKING, LOADING/UNLOADING, AND STORAGE WILL NOT BE ALLOWED.

D	RETAINING WALL (TYP.)	AB	DISTRICT MONUMENT SIGN
P	TRASH PICKUP LOCATION	AC	INTEGRAL CURB (SEE DETAIL) (TYP.)
D	MONUMENT SIGN	AD	PLANTED AREA (SEE LANDSCAPE PLAN) (TYP.)
R	MOVE IN/OUT AREA	AE	PEDESTRIAN CROSSWALK MARKING- TYPE B
S	CROSS WALK STRIPPING	AF	CONCRETE JERSEY BARRIER (TYP.)
T	STEPS AND HANDRAIL	AG	NOT USED
U	ORNAMENTAL FENCE (TYP.)	AH	NOT USED
V	CURB TRANSITION (SEE GRADING) (TYP.)	AI	NOT USED
W	FLUSH CURB(TYP.)	AJ	NOT USED
X	INSTALL SALVAGED PAVERS(TYP.)	AK	LIGHTPOLE MUST EXTEND OVER CROSSWALK
Y	RAISED CURBLESS SIDEWALK		
Z	5' TALL FENCE (SEE DETAIL)		
AA	GREEN SCREEN/ PLANTER WALL (SEE ARCHITECTUE)		

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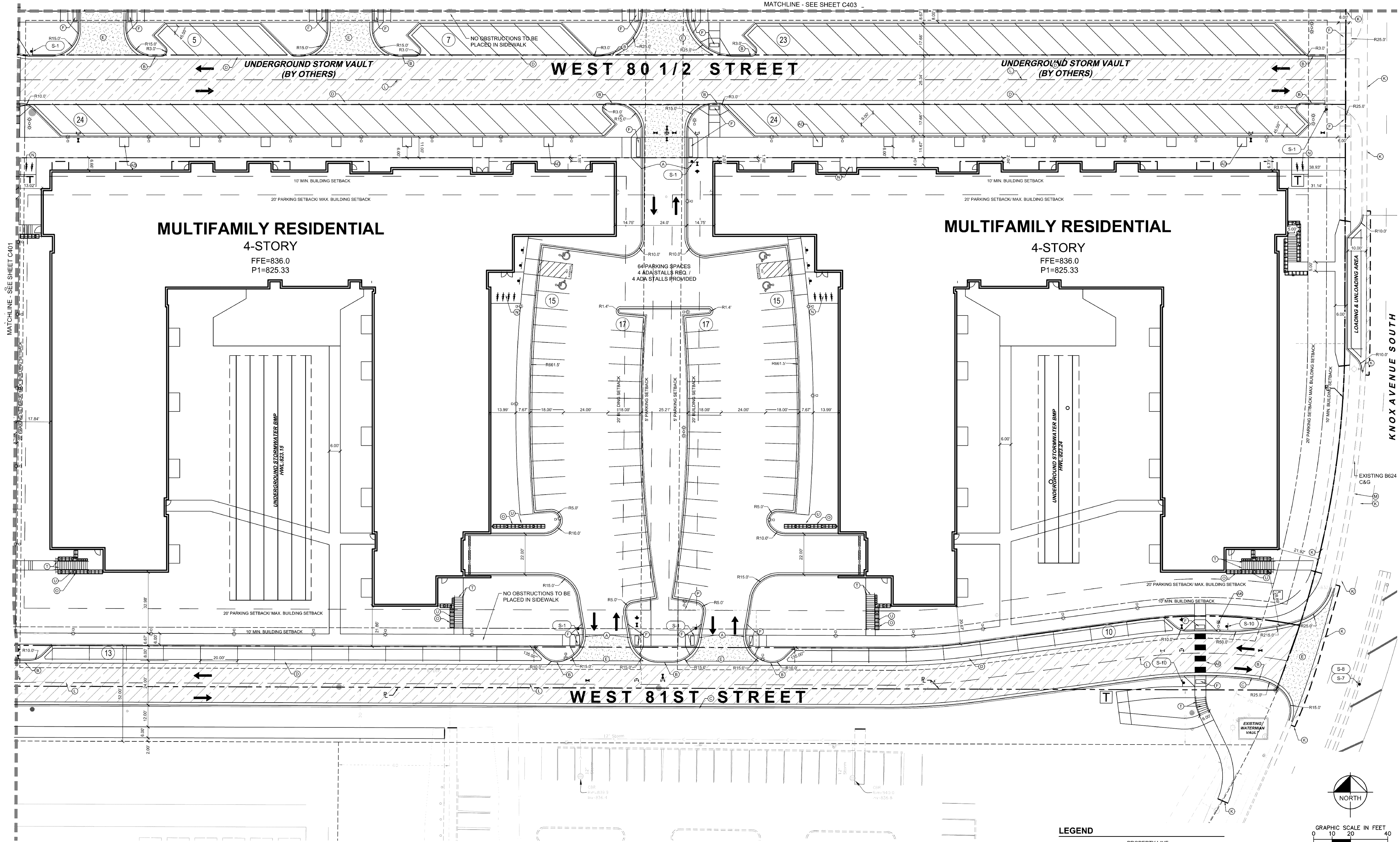
ENGINEERING DIVISION
Approved By: Julie Long, PE
09/04/2019 7:03:04 AM

APPROVED
ZONING REVIEW ONLY
NMJ 11/14/2019



8/29/2019 3:25 PM

MATCHLINE - SEE SHEET C401



SEE SHEET C401 FOR SITE NOTES.

KEYNOTE LEGEND

- (A) B612 CURB & GUTTER (SEE DETAIL) (TYP.)
- (B) B618 CURB & GUTTER (SEE DETAIL) (TYP.)
- (C) B1018 CURB & GUTTER (SEE DETAIL) (TYP.)
- (D) 26" RIBBON CURB (SEE DETAIL) (TYP.)
- (E) CONCRETE DRIVEWAY APRON (SEE DETAIL) (TYP.)
- (F) PUBLIC PEDESTRIAN CURB RAMP (WITH TRUNCATED DOMES) (SEE DETAIL) (TYP.)
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- (H) PRIVATE PEDESTRIAN CURB RAMP (WITHOUT TRUNCATED DOMES) (SEE DETAIL) (TYP.)
- (I) PRIVATE ACCESSIBLE DROPPED PEDESTRIAN RAMP (WITHOUT TRUNCATED DOMES) (SEE DETAIL) (TYP.)
- (J) ACCESSIBLE PARKING LOGO, STRIPING, & SIGNAGE (SEE DETAIL) (TYP.)
- (K) SAWCUT & MATCH EXISTING EDGE OF PAVEMENT/ CURB & GUTTER
- (L) PAVEMENT MARKING- 4" DASHED (10' LONG, 20' GAP), COLOR YELLOW
- (M) B624 CURB & GUTTER (SEE DETAIL) (TYP.)
- (N) BIKE RACK (SEE DETAIL) (TYP.)

- (O) RETAINING WALL (TYP.)
- (P) TRASH PICKUP LOCATION
- (Q) MONUMENT SIGN
- (R) MOVE IN/OUT AREA
- (S) CROSS WALK STRIPPING
- (T) STEPS AND HANDRAIL
- (U) ORNAMENTAL FENCE (TYP.)
- (V) CURB TRANSITION (SEE GRADING) (TYP.)
- (W) FLUSH CURB(TYP.)
- (X) INSTALL SALVAGED PAVERS(TYP.)
- (Y) RAISED CURBLESS SIDEWALK
- (Z) 5' TALL FENCE (SEE DETAIL)
- (AA) GREEN SCREEN/ PLANTER WALL (SEE ARCHITECTURE)

- (AB) DISTRICT MONUMENT SIGN
- (AC) INTEGRAL CURB (SEE DETAIL) (TYP.)
- (AD) PLANTED AREA (SEE LANDSCAPE PLAN) (TYP.)
- (AE) PEDESTRIAN CROSSWALK MARKING- TYPE B
- (AF) CONCRETE JERSEY BARRIER (TYP.)
- (AG) NOT USED
- (AH) NOT USED
- (AI) NOT USED
- (AJ) NOT USED
- (AK) LIGHTPOLE MUST EXTEND OVER CROSSWALK

SIGN LEGEND

KEYNOTE	NAME	MNDOT SIGN NUMBER	SIGN SIZE
S-1	STOP SIGN	R1-1	30" X 30"
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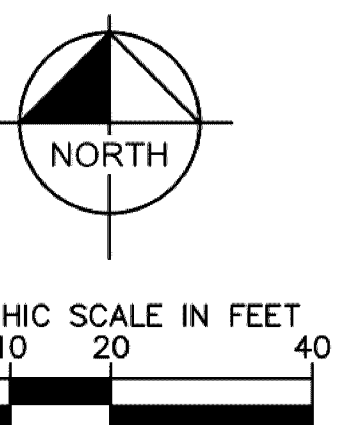
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- PROPERTY LINE
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- EASEMENT LINE
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- RETAINING WALL (TYP.)
- PROPOSED CURB AND GUTTER (TYP.)
- PROPOSED LIGHTPOLE (TYP.)
- PROPOSED SIGN (TYP.)
- PROPOSED TRANSFORMER (TYP.)
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- PROPOSED UNDERGROUND STORM VAULT (TYP.)
- PROPOSED PAVERS

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ENGINEERING DIVISION
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SITE PLAN - 80 1-2 & 81 ST STREET

C402

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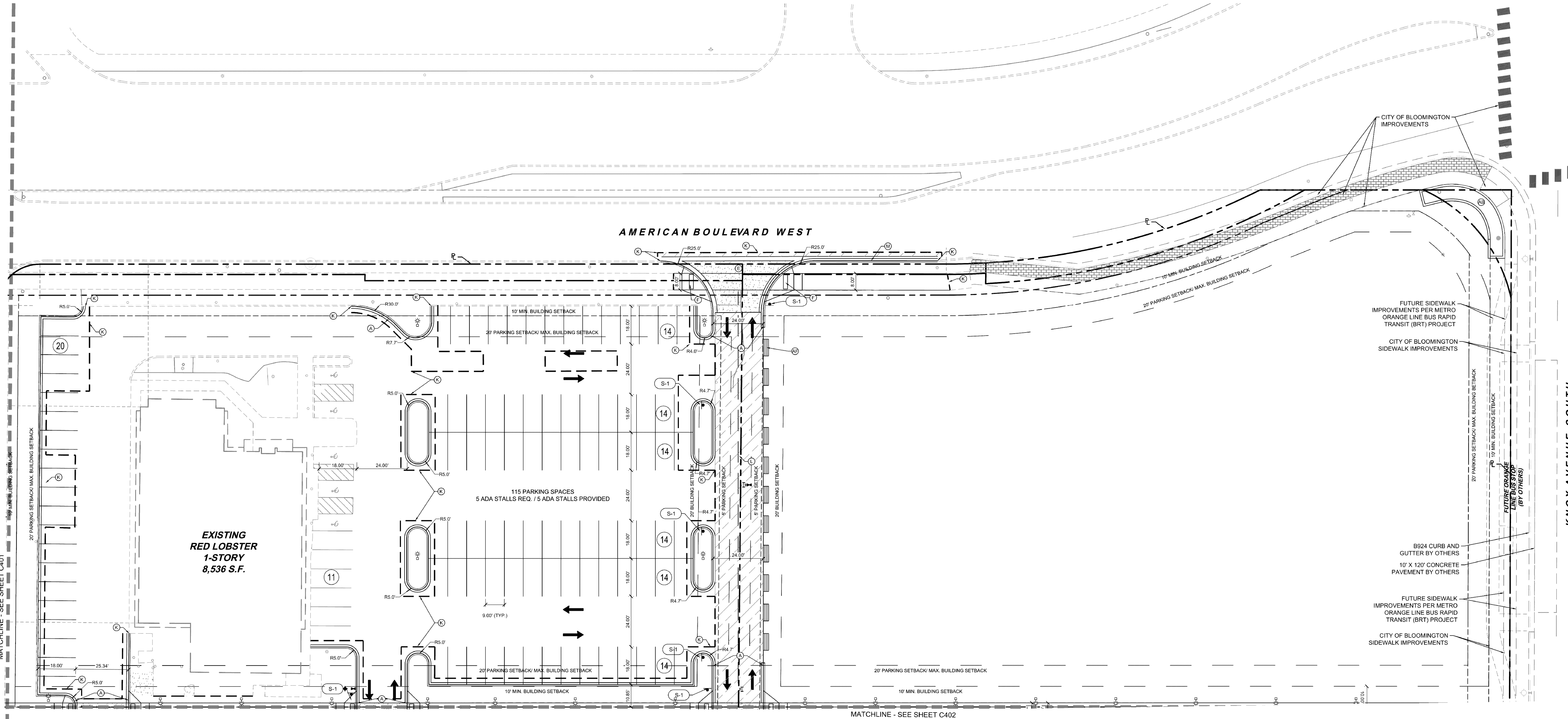
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THE DISTRICT

SITE PLAN - AMERICAN
BOULEVARD

C403



LEGEND

	PROPERTY LINE
	SETBACK LINE
	PROPOSED FENCE (TYP.)
	EASEMENT LINE
	SAWCUT LINE
	RETAINING WALL (TYP.)
	PROPOSED CURB AND GUTTER (TYP.)
	PROPOSED LIGHTPOLE (TYP.)
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	PROPOSED UNDERGROUND STORM VAULT (TYP.)
	PROPOSED PAVERS

SEE SHEET C401 FOR SITE NOTES.

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(P)	TRASH PICKUP LOCATION
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(R)	MOVE IN/OUT AREA
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(ES-1)	EXISTING STOP SIGN TO REMAIN	-	-

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